

12/19/2025 11:30:59 AM C:\PROGRAMDATA\BENTLEY\OPENROADS DESIGNER 2023.00\CONFIGURATION\WORKSPACES\TDOT\_STANDARDS\WORKSETS\132483.00-SHELBY\_FAYETTE COUNTY - I269\DGN\241269-SHT-SIGNATURE.DGN



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

**Eric S. Brown** Digitally signed by Eric S. Brown  
Date: 2025.12.19 12:13:04 -06'00'

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TENNESSEE DEPARTMENT OF TRANSPORTATION  
ADMINISTRATION BUILDING, FIRST FLOOR  
300 BENCH MARK PLACE  
JACKSON, TN 38301  
ERIC S. BROWN, P.E. NO. 120440

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET .....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, & STANDARD	
TRAFFIC DESIGN DRAWINGS .....	1A
PROJECT COMMITMENTS .....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B, 2B1, 2B2
GENERAL NOTES .....	2C
SPECIAL NOTES .....	2D
ENVIRONMENTAL NOTES .....	2E, 2E1
TABULATED QUANTITIES .....	2F
UTILITY NOTES AND UTILITY OWNERS .....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL .....	T1

YEAR	PROJECT NO.	SHEET NO.
2026	NH-I-269(43)	ROADWAY-SIGN1

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SIGNATURE  
SHEET

Index Of Sheets  
SEE SHEET NO. 1A

# STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF ENGINEERING

PROJECT TO BE LET WITH  
FAYETTE COUNTY, I-269  
PIN: 132482.00

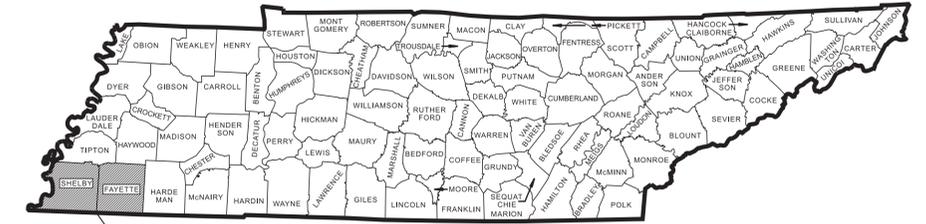
## FAYETTE & SHELBY COUNTIES

I-269  
FROM FAYETTE COUNTY LINE (SHELBY SEQUENCE 2)  
THROUGH FAYETTE AND SHELBY COUNTY  
TO SR-193 IN FAYETTE COUNTY  
RESURFACE & SAFETY  
MILL, CM, OGFC, BRIDGE REPAIR, GUARDRAIL, & PAVEMENT MARKINGS

STATE HIGHWAY NO. N/A F.A.H.S. NO. I-269

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2026	1
FED. AID PROJ. NO.	NH-I-269(43)	
STATE PROJ. NO.	24I269-F8-007	
STATE PROJ. NO.	79I269-F8-007	
STATE PROJ. NO.	24I269-M3-009	
STATE PROJ. NO.	79I269-M3-007	



PROJECT LOCATION

BRIDGE ID. #	79I02690005	79I02690007	79I02690008
	79I02690009	79I02690010	79I02690011
	79I02690013	79I02690015	79I02690016
	24I02690011	24I02690013	24I02690015
	24I02690017	24I02690018	24I02690019
	24I02690021	24I02690022	24I02690023
	79SR1930001		

NO EXCLUSIONS

24I269-M3-009  
BRIDGE REPAIR PROJECT NO. 79I269-M3-007  
BRIDGE #79I02690016 L.M. 2.12 BRIDGE #79I02690015 L.M. 2.12  
BRIDGE #79I02690009 L.M. 1.32 BRIDGE #79I02690010 L.M. 1.32  
BRIDGE #24I02690017 L.M. 1.57 BRIDGE #24I02690018 L.M. 1.57  
BRIDGE #24I02690021 L.M. 2.59 BRIDGE #24I02690022 L.M. 2.59

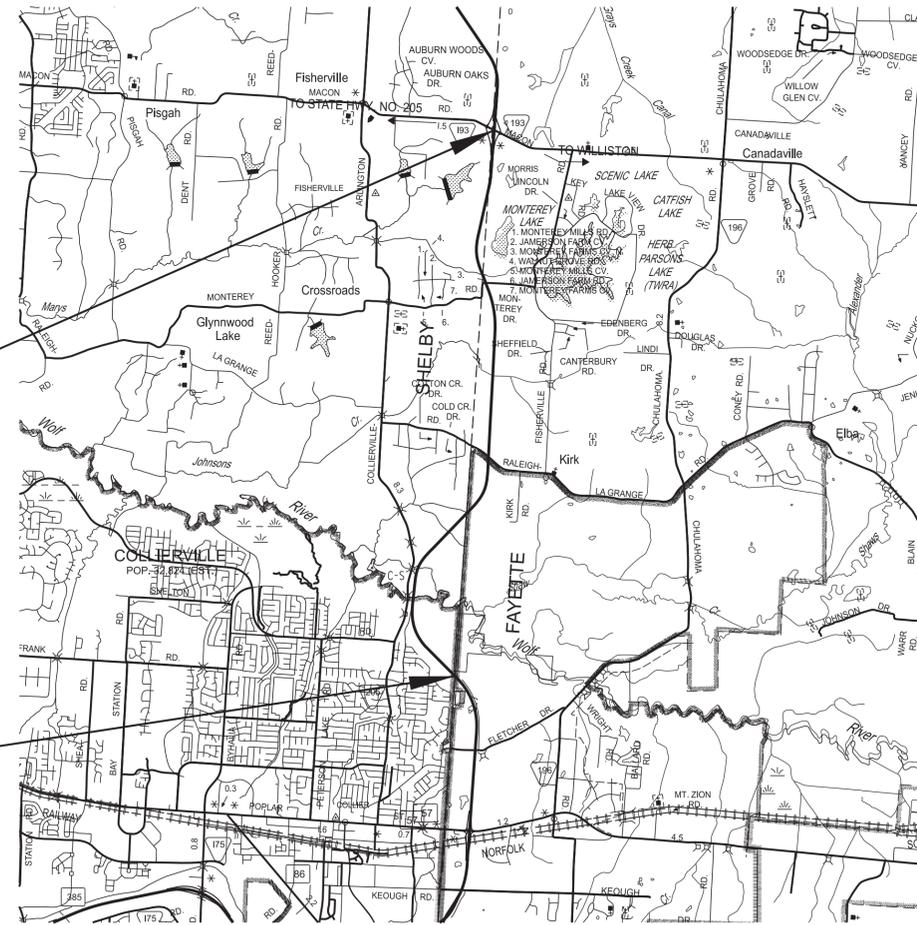
24I269-F8-007  
79I269-F8-007  
END PROJECT NO. NH-I-269(43) RESURFACE & SAFETY  
S.R. 193 IN FAYETTE COUNTY (L.M. 0.63)

24I269-F8-007  
79I269-F8-007  
BEGIN PROJECT NO. NH-I-269(43) RESURFACE & SAFETY  
FAYETTE COUNTY LINE (SHELBY SEQUENCE 2) (L.M. 0.00)

### SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.



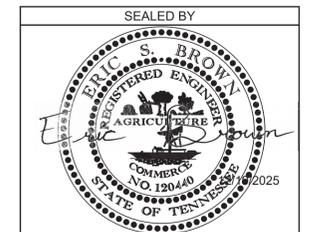
SCALE: 1"= 5280'



PROJECT LENGTH 6.46 MILES  
TOTAL LANE MILES RESURFACED 22.88 MILES

TRAFFIC COUNTER AND WEATHER STATION	
STATION	LOG MILE
TCS # 1051	0.323

TRAFFIC DATA	
ADT (2026)	24609
POSTED SPEED	65 MPH



APPROVED:   
SHANE HESTER, CHIEF ENGINEER

DATE: \_\_\_\_\_  
APPROVED:   
WILL REID, COMMISSIONER

TDOT PROJECT MANAGER: LYNN EVANS, P.E., REG. 4

DESIGNER: ALFRED WILLIS JR., REG. 4

CHECKED BY: ERIC S. BROWN, P.E., REG. 4

P.E. NO. 98046-4175-04

PIN NO. 132483.00

# ROADWAY INDEX

## SHEET NAME

## SHEET NO.

SIGNATURE SHEETS .....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, & STANDARD TRAFFIC DESIGN DRAWINGS .....	1A
PROJECT COMMITMENTS .....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B, 2B1, 2B2
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SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES .....	2F
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1
BRIDGE REPAIR PLANS .....	B-1
NOTE: THE ALPHABETICAL LETTERS "I", "O" & "Q" ARE NOT USED IN THE NUMBERING OF SHEETS.	

# STANDARD ROADWAY DRAWINGS

## DWG. REV. DESCRIPTION

### STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS

RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

### SAFETY DESIGN AND GUARDRAILS

S-GR31-1	03-13-25	GUARDRAIL DETAILS
S-GR31-1A	06-28-19	GUARDRAIL AND BLOCK-OUT DETAILS
S-GR31-1B		GUARDRAIL FASTENING HARDWARE
S-GR31-1C	07-07-23	GUARDRAIL GENERAL NOTES AND POST DETAILS
S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL

# STANDARD TRAFFIC DESIGN DRAWINGS

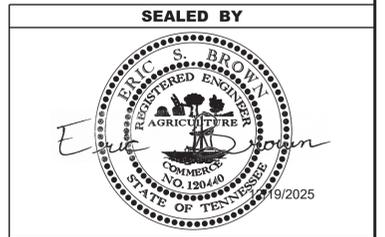
## DWG. REV. DESCRIPTION

### DESIGN - TRAFFIC CONTROL

T-M-5	01-24-25	MARKING DETAIL FOR FREEWAYS
T-M-6	01-24-25	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-7	01-24-25	GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-8	01-24-25	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-15	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
T-WZ-11	03-26-25	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-26-25	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-60	03-26-25	FREEWAY RESURFACING SIGNING PROCEDURES
T-WZ-63	03-26-25	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64	03-26-25	WORK ZONE IN THE VICINITY OF AN EXIT

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	1A

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**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

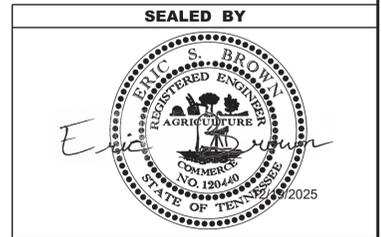
ROADWAY INDEX,  
STANDARD ROADWAY  
DRAWINGS, & STANDARD  
TRAFFIC DESIGN DRAWINGS

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## PROJECT COMMITMENTS

COMMITMENT ID	SOURCE DIVISION	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	Asbestos Containing Material (ACM) surveys were completed on the following bridges and no asbestos was detected. Please see the reports for further details and photographs. No special accommodations for demolition and waste disposal are anticipated for these structures and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03)	BRIDGES
EDHZ002	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	Bridge No. 79I02690007 I-269 NB over Wolf River LM 0.60 (19-SR045-17.92R) Bridge No. 79I02690008 I-269 SB over Wolf River LM 0.60 (19-SR045-17.92L) Bridge No. 79I02690009 I-269 NB over Wolf River Overflow LM 1.33 (79-I0269-01.33R) Bridge No. 79I02690010 I-269 SB over Wolf River Overflow LM 1.33 (79-I0269-01.33L) Bridge No. 79I02690015 I-269 NB over Tributary to Wolf River LM 2.13 (79-I0269-02.13R) Bridge No. 79I02690016 I-269 SB over Tributary to Wolf River LM 2.13 (79-I0269-02.13L) Bridge No. 24I02690017 I-269 NB over Johnson Creek LM 1.57 (24-I0269-01.57R) Bridge No. 24I02690018 I-269 SB over Johnson Creek LM 1.57 (24-I0269-01.57L) Bridge No. 24I02690021 I-269 NB over Monterey Rd LM 2.59 (24-I0269-02.59R) Bridge No. 24I02690022 I-269 SB over Monterey Rd LM 2.59 (24-I0269-02.59L)	BRIDGES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	1B



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

PROJECT  
COMMITMENTS

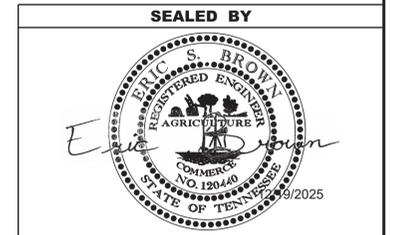
### ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY 24I269-F8-007	QUANTITY 79I269-F8-007	TOTAL QUANTITY
	REMOVAL OF ASPHALT PAVEMENT	S.Y.	1840	1200	3040
	WATER	M.G.	10	7	17
(1)	BROOMING & DEGRASSING SHOULDERS	L.M.	13.08	9.8	22.88
	MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	1299	971	2270
(2)	ASPHALT CONCRETE MIX (PG64-22) GRADING CM	TON	3076	2276	5352
	ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING A	TON	423	276	699
	ASPHALT CONCRETE MIX (PG76-22) GRADING CM	TON	8613	6372	14985
	TRACKLESS TACK COAT	TON	65	48	113
(3)(4)	LONGITUDINAL JOINT SEALANT	L.M.	13.08	9.8	22.88
(5)(6)	ACS MIX(PG76-22) GRADING D	TON	423	280	703
(7)	ACS MIX (PG76-22) OGFC	TON	7702	5697	13399
	SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	13.08	9.8	22.88
(8)	COLD PLANING BITUMINOUS PAVEMENT	TON	12090	8704	20794
(9)	GUARDRAIL TRANSITION 27IN TO 31IN	EACH	2		2
(9)	TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH	2		2
(9)	GUARDRAIL REMOVED	L.F.	100		100
(10)(11)	TRAFFIC CONTROL	LS	0.35	0.35	0.7
	FLEXIBLE DRUMS (CHANNELIZING)	EACH	507	482	989
(12)	WARNING LIGHTS (TYPE C)	EACH	35	35	70
(9)(13)	SIGNS (CONSTRUCTION)	S.F.	1197	1120	2317
(14)	SIGNS (CONSTRUCTION)(REDUCED SPEED WARNING)	EACH	2	2	4
	ARROW BOARD (TYPE C)	EACH	2	2	4
	SPEED FEEDBACK SIGN ASSEMBLY	EACH	1	1	2
	DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	10	8	18
	QUEUE PROTECTION TRUCK	DAY	41	39	80
	CHANGEABLE MESSAGE SIGN UNIT	EACH	1	1	2
	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	530	410	940
(15)(16)	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	530	410	940
	PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	1031		1031
(17)	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	29.43	22.05	51.48
(15)	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	14.96	14.11	29.07
(15)	ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	2130		2130
(15)	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	290		290
	MOBILIZATION	LS	0.35	0.35	0.7
	ALTERNATE AA1				
(18)	HOT APPLIED TACK COAT	TON	90	67	157
	ALTERNATE AA2				
(19)	TRACKLESS TACK COAT	TON	119	88	207

### FOOTNOTES

- (1) INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET NO. 2C, FINAL PAVEMENT MARKING NOTE (6) FOR MORE INFORMATION.
- (2) TO BE USED FOR BREAKOUT LOCATIONS ONLY.
- (3) USE CRAFCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO.
- (4) TO BE USED FOR SEALING OF TRANSVERSE AND LONGITUDINAL JOINTS IN ALL PAVEMENT LAYERS, EXCEPT OGFC, ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER.
- (5) TO BE USED AT BRIDGE APPROACHES.
- (6) INCLUDES 143 TONS FOR GORE AREAS.
- (7) 198 TONS TO BE USED FOR PLANT STARTUP AND HEATING UP EQUIPMENT AT BEGINNING OF EACH SHIFT, 15 TONS WASTE MATERIAL PER DAY.
- (8) INCLUDES 2873 TONS FOR GORE AREAS, BRIDGE TRANSITIONS, AND PROJECT LIMITS.
- (9) SEE TABULATED QUANTITIES ON SHEET 2F.
- (10) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- (11) THIS ITEM INCLUDES TRAFFIC CONTROL FOR ANY BRIDGE REPAIR WORK. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- (12) TO BE USED FOR TRANSITION TAPERS.
- (13) THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION).
- (14) ITEM TO BE USED ONLY WHEN A REDUCED SPEED LIMIT IS ESTABLISHED WITHIN THE PROJECT CONSTRUCTION WORK ZONE LIMITS. ITEM INCLUDES SIGN FACE, SUPPORTS, AND TWO TYPE "B" FLASHERS PER THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TURNING ON THE TYPE "B" FLASHERS WHEN WORKERS ARE IN THE CONSTRUCTION WORK ZONE AND TURNING THEM OFF WHEN WORKERS ARE NO LONGER IN THE CONSTRUCTION WORK ZONE.
- (15) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (16) ITEM TO BE USED AS PERMANENT MARKING ONLY.
- (17) ITEM TO BE USED AS TEMPORARY MARKING ONLY.
- (18) TO BE USED FOR TACK COAT UNDERNEATH OGFC. HOT APPLIED TRACKLESS TACK MAY BE EITHER HOT APPLIED ASPHALT BINDER OR AN APPROVED HOT APPLIED TRACKLESS TACK COAT FROM QPL 40, SECTION F. IF USING ASPHALT BINDER THE MINIMUM GRADE SHALL BE PG64-22 BUT HIGHER GRADE MAY BE USED AT THE CONTRACTOR'S DISCRETION.
- (19) USE AN APPROVED TRACKLESS TACK FROM QPL 40-F APPLIED WITH A DISTRIBUTOR AT A MINIMUM RATE OF 0.20 GAL/SY (APPROXIMATE RESIDUAL RATE 0.10 GAL/SY). PAVING OF THE OGFC SHALL NOT BEGIN UNTIL THE DEPARTMENT IS SATISFIED THE APPLICATION RATE IS ACHIEVED AND THE EMULSION HAS FULLY BROKEN. MULTIPLE PASSES MAY BE REQUIRED. OR, EMULSION TYPE CQS-1HP MAY BE APPLIED WITH A SPRAY PAVER AT AN APPLICATION RATE BETWEEN 0.18 TO 0.23 GAL/SY. THE SPRAY PAVER SHALL BE A SINGLE PIECE OF EQUIPMENT THAT APPLIES THE TACK COAT AND SPREADS THE BITUMINOUS PAVEMENT. AT A MINIMUM THE SPRAY PAVER SHALL MEET THE PAVER REQUIREMENTS OF 407.06 AND THE DISTRIBUTOR REQUIREMENTS IN 402.03.

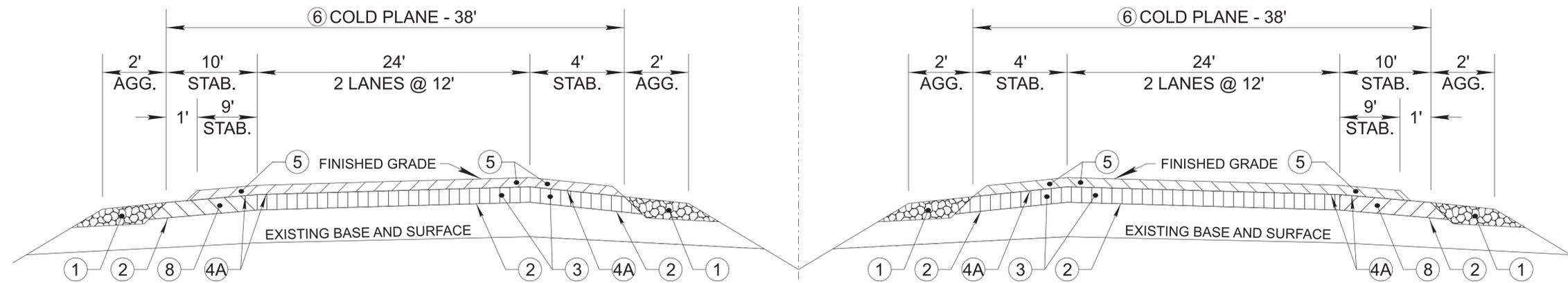
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2



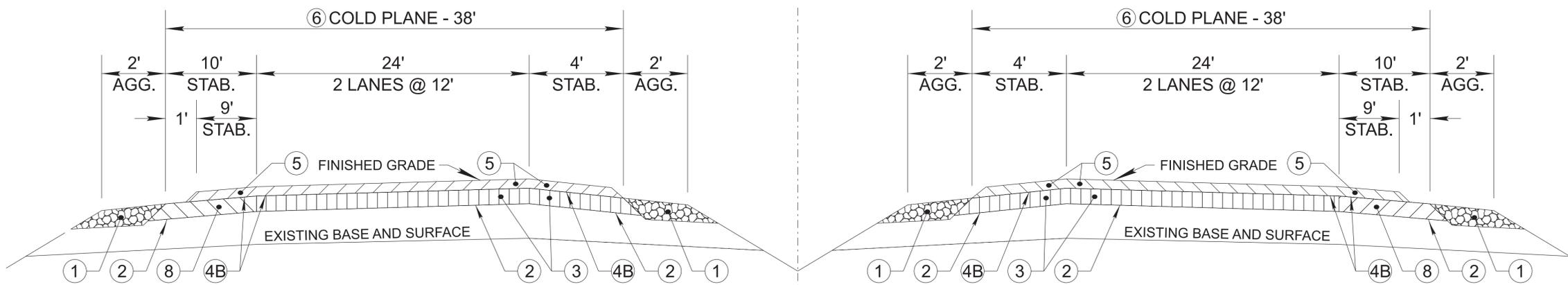
**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

**ESTIMATED  
ROADWAY  
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2B



I-269  
ALTERNATE AA1



I-269  
ALTERNATE AA2

SHELBY COUNTY SEQUENCE 2 FROM: L.M. 0.00 TO: L.M. 2.16  
 FAYETTE COUNTY SEQUENCE 3 FROM: L.M. 0.00 TO: L.M. 2.70  
 SHELBY COUNTY SEQUENCE 3 FROM: L.M. 0.00 TO: L.M. 0.98  
 FAYETTE COUNTY SEQUENCE 4 FROM: L.M. 0.00 TO: L.M. 0.63

SEE SHEET 2B2 FOR PAVEMENT SCHEDULE

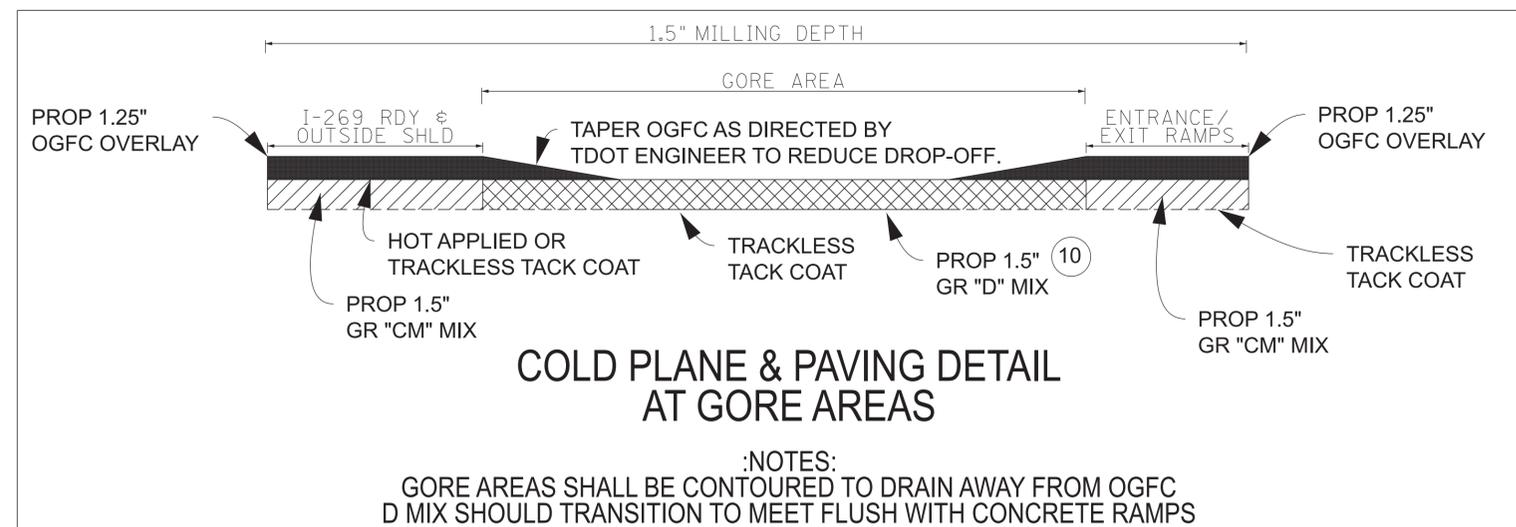
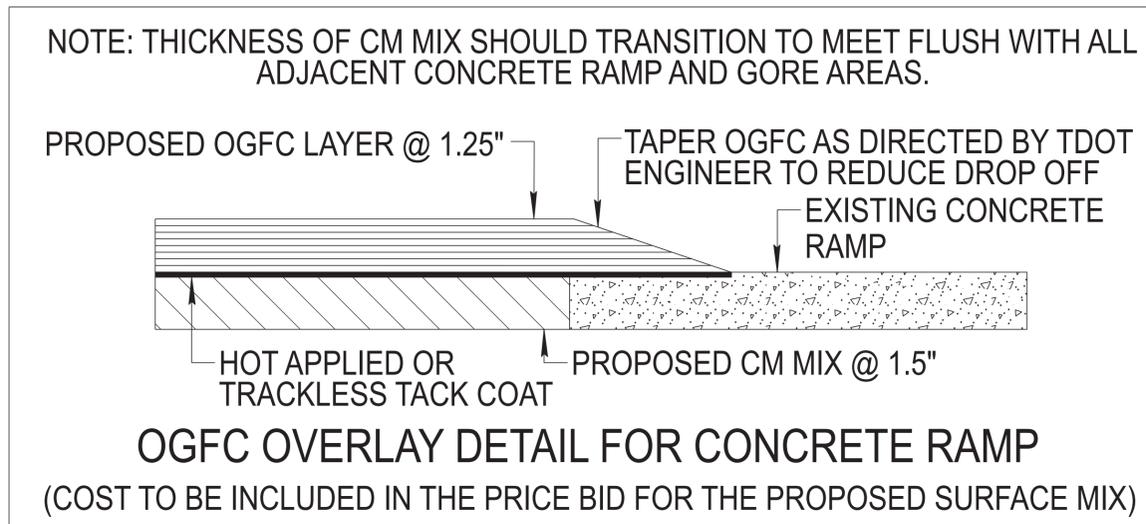
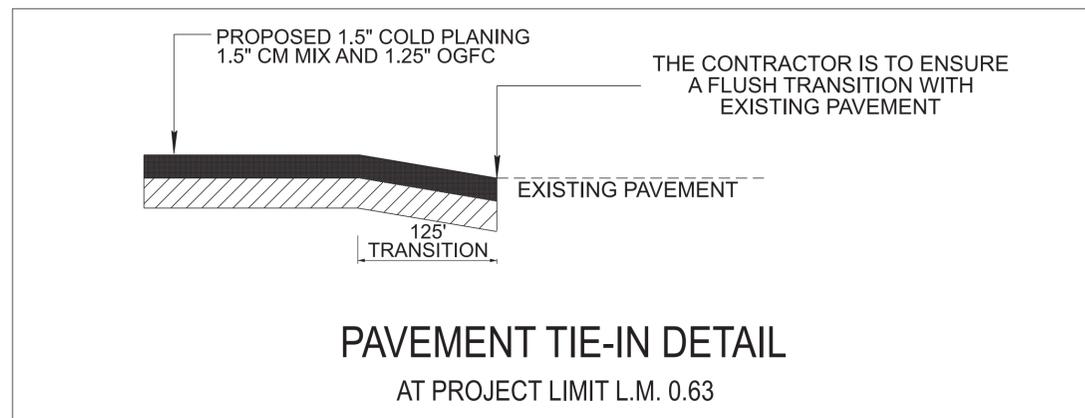
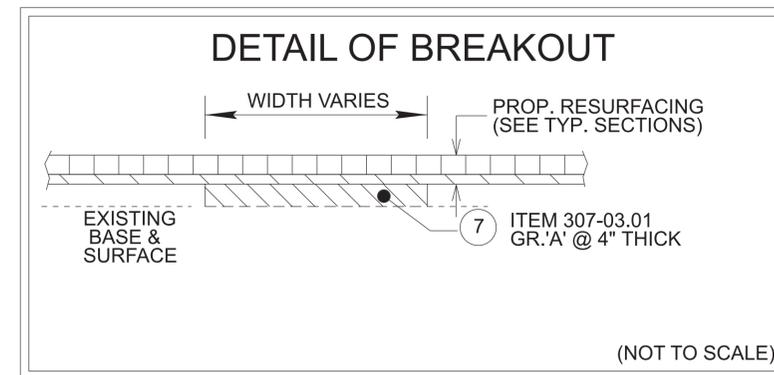
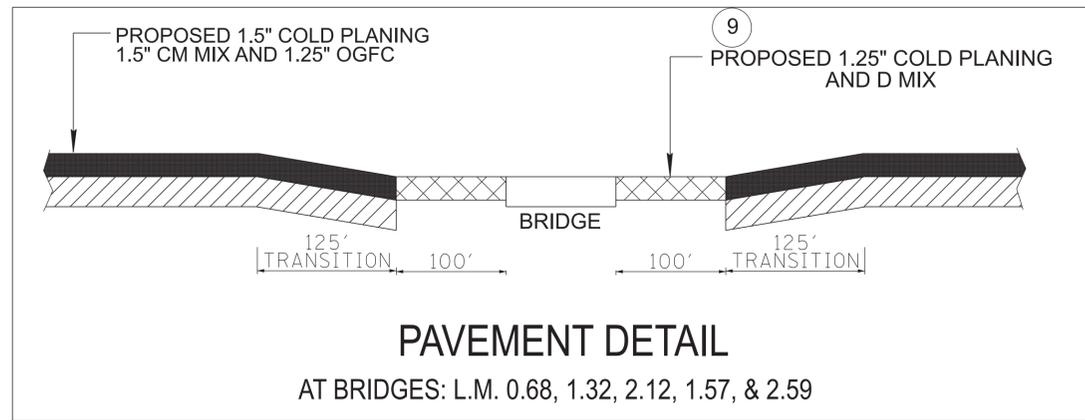
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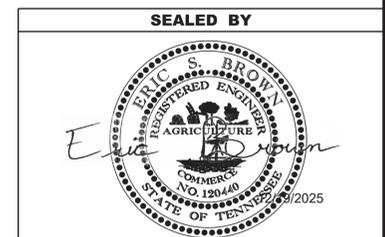
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS AND  
PAVEMENT  
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2B1



**SEE SHEET 2B2 FOR PAVEMENT SCHEDULE**

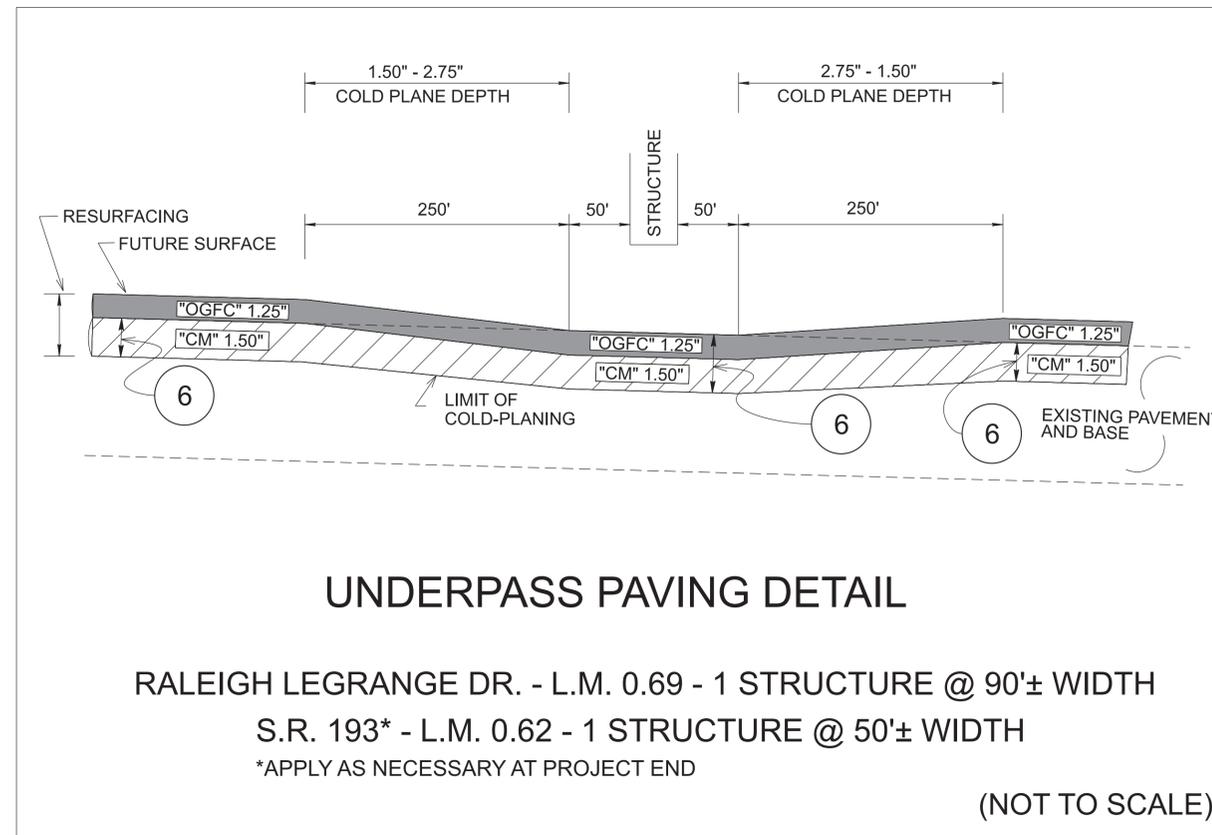


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS AND  
PAVEMENT  
SCHEDULE

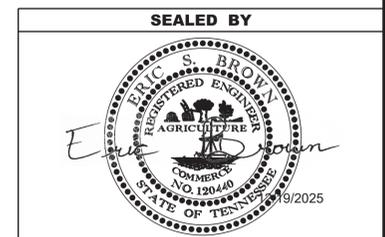
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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2B2



### PROPOSED PAVEMENT SCHEDULE

① MINERAL AGGREGATE BASE @ 1.50"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING (C OR D)	⑥ COLD PLANING @ 1.50"± THICK (APPROX. 157.50 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
② TRACKLESS TACK COAT 403-02.01 TRACKLESS TACK COAT SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD.	* COLD PLANING @ 1.25"± THICK (APPROX. 157.50 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT (THIS ITEM IS TO BE USED FOR BRIDGE APPROACHES)
③ BITUMINOUS COURSE @ 1.50"± THICK (APPROX. 165.00 LBS./S.Y.) ITEM 307-03.12 ASPHALT CONCRETE MIX (PG76-22) GRADING CM	⑦ BITUMINOUS COURSE @ 4"± THICK (APPROX. 460.00 LBS./S.Y.) ITEM 307-03.01 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING A (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)
④A (ALTERNATE AA1) BITUMINOUS MATERIAL FOR HOT APPLIED TACK COAT 403-02.02 HOT APPLIED TACK COAT APPLICATION RATE OF 0.13-0.18 GAL./S.Y.	⑧ BITUMINOUS COURSE @ 1.50"± THICK (APPROX. 165.00 LBS./S.Y.) ITEM 307-01.18 ASPHALT CONCRETE MIX (PG64-22) GRADING CM
④B (ALTERNATE AA2) TRACKLESS TACK COAT 403-02.01 TRACKLESS TACK COAT APPLICATION RATE OF APPROX. 0.20 GAL/SY	⑨ BITUMINOUS COURSE @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.) ITEM 411-03.10 ASPHALT CONCRETE MIX (PG76-22) GRADING D (THIS ITEM IS TO BE USED FOR BRIDGE APPROACHES)
⑤ ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 110.00 LBS./S.Y.) ITEM 411-03.23 ACS MIX (PG76-22) OGFC	⑩ BITUMINOUS COURSE @ 1.50"± THICK (APPROX. 159.00 LBS./S.Y.) ITEM 411-03.10 ASPHALT CONCRETE MIX (PG76-22) GRADING D (THIS ITEM IS TO BE USED FOR GORE AREAS AT EXIT/ENTRANCE RAMPS)



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

TYPICAL  
SECTIONS

# GENERAL NOTES

## GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

## GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

## PAVEMENT MARKINGS

### TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

### FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
  - a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE ENGINEER.
  - b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE ENGINEER.
  - c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
  - d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PAVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PAVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

### SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

## PAVEMENT

### PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

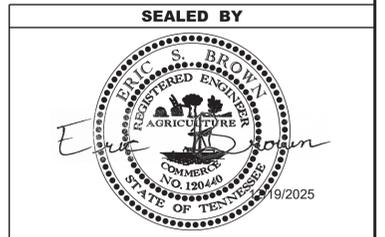
### RESURFACING

- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

## CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2C



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

GENERAL  
NOTES

# SPECIAL NOTES

## PAVEMENT

### RESURFACING

- (1) TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:
  - A. THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
  - B. THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.
  - C. THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS  $\geq 70,000$  OR WITHIN 96 HOURS IF THE CURRENT ADT IS  $< 70,000$ .
  - D. RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
  - E. ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS
  - F. IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.
  - G. ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.
- (2) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (3) THE INSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.
- (6) AT THE START OF EACH NIGHT'S PAVING PRODUCE APPROXIMATELY 15 TONS OF THE BITUMINOUS PAVEMENT FOR THE PURPOSE OF PLANT START UP AND HEATING UP THE MATERIAL TRANSFER DEVICE (MTD). THIS MATERIAL SHALL BE UNLOADED INTO THE MTD AT A MINIMUM TEMPERATURE OF 280F AND THE ENTIRE LOAD DISCHARGED THROUGH THE MTD AND WASTED OFF THE PROJECT SITE IMMEDIATELY PRIOR TO THE COMMENCING PAVING OPERATIONS."
- (7) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

## PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

## SIGNS

- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

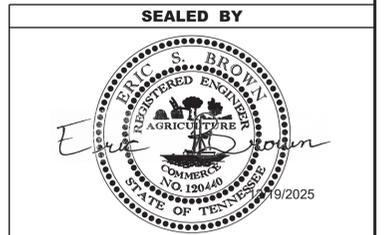
## CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (2) THE DEPARTMENT SHALL RESERVE THE RIGHT TO REOPEN LANES AS NECESSARY WHEN TRAFFIC CONDITIONS ARE DEEMED UNACCEPTABLE (EXCESSIVE QUEUE LENGTHS AND DELAY TIMES). THE CONTRACTOR SHALL BE REQUIRED TO FULLY COOPERATE WITH THE PROJECT SUPERVISOR WHEN REQUESTED TO MAKE CHANGES TO THE TRAFFIC CONTROL.
- (3) MESSAGE BOARDS SHALL BE USED NEAR INTERCHANGES AND/OR OTHER DESIGNATED AREAS IN ADVANCE OF THE WORK ZONE TO ALERT MOTORISTS OF POSSIBLE DELAYS AND RECOMMEND THE USE OF ALTERNATE ROUTES. THE MESSAGES SHALL BE UPDATED AS OFTEN AS NECESSARY SO THAT THE MOTORISTS OBTAIN CURRENT TRAFFIC INFORMATION. MESSAGE BOARDS SHALL BE RELOCATED AS NECESSARY AS THE WORK PROGRESSES. THE CONTRACTOR SHALL BE REQUIRED TO IDENTIFY AN INDIVIDUAL WORKING ON THE PROJECT THAT WILL BE RESPONSIBLE FOR KEEPING THE MESSAGES CURRENT AND RELOCATING MESSAGE BOARDS AS REQUESTED BY TDOT. THE PROJECT SUPERVISOR SHALL HAVE THE AUTHORITY TO APPROVE ALL MESSAGES AND REQUIRED CHANGES AT ANY TIME DUE TO CHANGING TRAFFIC CONDITIONS.

## JOINT SEALANTS

- (2) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN ALL PAVEMENT LAYERS EXCEPT OGFC SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (3) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (4) TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2D



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

SPECIAL  
NOTES

# ENVIRONMENTAL NOTES

## ENVIRONMENTAL GENERAL NOTES

### NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

### SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

### PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

## ENVIRONMENTAL SPECIAL NOTES

### ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

### ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRECONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

### PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

### SCOPE OF WORK

- (6) THIS PROJECT INCLUDES COLD PLANING, PAVING, STRIPING, TRAFFIC CONTROL, GUARDRAIL, AND BROOMING AND DEGRASSING.

## EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

### DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

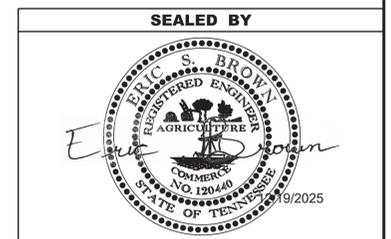
### SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

## GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2E



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

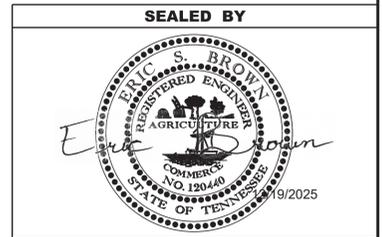
**ENVIRONMENTAL  
NOTES**

# ENVIRONMENTAL NOTES

## SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2E1



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

**ENVIRONMENTAL  
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	2F

### TRAFFIC CONTROL SIGN TABULATION

M.U.T.C.D. SIGN NO.	LEGEND	SIZE IN INCHES			S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	X	W			
G20-1	ROAD WORK NEXT 7 MILES	48"	X	24"	8	4	32.00
G20-2	END ROAD WORK	48"	X	24"	8	5	40.00
R1-2	YIELD	48"	X	48"	16	1	16.00
W4-1R	MERGING TRAFFIC	48"	X	48"	16	4	64.00
W4-2L	MERGE RIGHT SYMBOL	48"	X	48"	16	4	64.00
W4-2R	MERGE LEFT SYMBOL	48"	X	48"	16	4	64.00
W8-11	UNEVEN LANES	48"	X	48"	16	29	464.00
W8-15aP	MOTORCYCLE PLAQUE	30"	X	24"	5	29	145.00
W8-17	GROOVED PAVEMENT	48"	X	48"	16	29	464.00
W8-17P	SHOULDER DROP-OFF PLAQUE	48"	X	48"	16	28	448.00
W8-17P	SHOULDER DROP-OFF	30"	X	24"	5	28	140.00
W20-1	ROAD WORK AHEAD	48"	X	48"	16	5	80.00
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	X	48"	16	4	64.00
W20-5L	LEFT LANE CLOSED 1500 FT	48"	X	48"	16	4	64.00
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	X	48"	16	4	64.00
W20-5R	RIGHT LANE CLOSED 1500 FT	48"	X	48"	16	4	64.00
W21-2	FRESH OIL	48"	X	48"	16	4	64.00
W21-5	SHOULDER WORK	48"	X	48"	16	4	64.00
THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.						<b>TOTAL</b>	<b>2317</b>

### PROPOSED GUARDRAIL (RESURFACING)

SIDE			LOG MILE	GUARDRAIL		TERMINAL ANCHORS
				GUARDRAIL TRANSITION 27 IN TO 31 IN	GUARDRAIL REMOVED	TYPE 38 MASH TL3 (26.896') 705-06.20 (EACH)
LT	M	RT	FAYETTE COUNTY SEQUENCE 3	705-02.10 EACH	706-01 (L.F.)	
	X		0.645	1.000	50	1
		X	1.488	1.000	50	1
<b>TOTALS</b>				<b>2.000</b>	<b>100</b>	<b>2</b>

### BRIDGE NOTES

	BRIDGE NO.	LENGTH (FT.)	TREATMENT
L.M. 0.02R	79I02690005	26.3	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 0.02L	79I02690005	26.3	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 0.68R	79I02690007	3250	LEAVE AS IS
L.M. 0.68L	79I02690008	3250	LEAVE AS IS
L.M. 1.32R	79I02690009	177	SEE BRIDGE REPAIR PLANS
L.M. 1.32L	79I02690010	177	SEE BRIDGE REPAIR PLANS
L.M. 1.61R	79I02690011	22.3	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 1.61L	79I02690011	22.3	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 1.87R	79I02690013	20.9	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 1.87L	79I02690013	20.9	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 2.12R	79I02690015	240	SEE BRIDGE REPAIR PLANS
L.M. 2.12L	79I02690016	240	SEE BRIDGE REPAIR PLANS
L.M. 0.26R	24I02690011	31.7	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 0.26L	24I02690011	31.7	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 0.56R	24I02690013	36.6	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 0.56L	24I02690013	36.6	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 0.69	24I02690015		TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
L.M. 1.57R	24I02690017	120	SEE BRIDGE REPAIR PLANS
L.M. 1.57L	24I02690018	120	SEE BRIDGE REPAIR PLANS
L.M. 1.77R	24I02690019	22.4	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 1.77L	24I02690019	22.4	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 2.59R	24I02690021	184	SEE BRIDGE REPAIR PLANS
L.M. 2.59L	24I02690022	184	SEE BRIDGE REPAIR PLANS
L.M. 2.70R	24I02690023	30	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 2.70L	24I02690023	30	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 0.62	79SR1930001		TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT

SEALED BY



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TABULATED  
QUANTITIES

12/19/2025 11:38:30 AM C:\PROGRAMDATA\BENTLEY\OPENROADS DESIGNER 2023.00\CONFIGURATION\WORKSPACES\TDOT\_STANDARD\WORKSETS\132483.00-SHELBY\_FAYETTE COUNTY - I269\IDGN\24269-SHT-UTILITY NOTES.DGN

### UTILITY

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

### UTILITY OWNERS

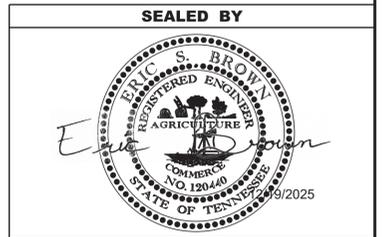
**GAS:**  
**HARDEMAN/FAYETTE**  
 15175 HWY 57 EAST  
 P.O. BOX 7  
 MOSCOW, TN 38057  
 CONTACT: CLAY JOYNER  
 OFFICE PHONE: 901 877 6236  
 Email: CJOYNER@HFUTILITYDISTRICT.COM

**ELECTRIC:**  
**CHICKASAW ELECTRIC CO.**  
 17970 HWY 64 EAST  
 SOMERVILLE, TN 38068  
 CONTACT: LOYD MUNCY  
 OFFICE PHONE: 901 466 2536  
 Email: LMUNCY@CHICKSAW.COOP

**WATER/GAS/ELECTRIC:**  
**MLG&W**  
 220 SOUTH MAIN ST.  
 MEMPHIS, TN, 38103  
 CONTACT: DARRYL MCLEMORE  
 OFFICE PHONE: 901 528 4186  
 CELL PHONE: 901 502 6207  
 Email: DCMCLEMORE@MLGW.ORG

**TELEPHONE:**  
**AT&T**  
 315 E. COLLEGE ST.  
 JACKSON, TN 38301  
 CONTACT: DANIEL R. POTTS  
 OFFICE PHONE: 901 488 2359  
 Email: DP7607@ATT.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-I-269(43)	3



**STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION**

UTILITY NOTES  
 AND  
 UTILITY OWNERS

# PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	NH-1-269(43)	T1

## A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
  - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
  - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
  - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
  - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
  - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
    - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
    - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
  - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
  - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

## 3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
  - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
  - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO 6 INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

## 4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

## B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.
  - a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:
  - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
    - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
    - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
  - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
    - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
    - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
  - b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

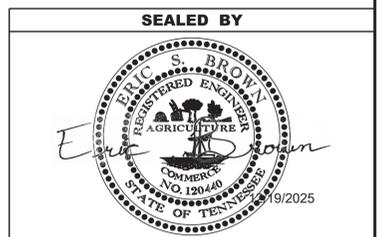
THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

## C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

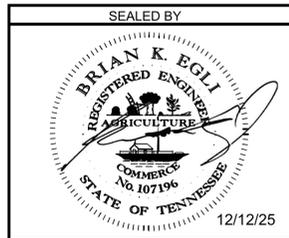
PAVEMENT EDGE  
DROP-OFF NOTES  
FOR  
TRAFFIC CONTROL



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79I269-M3-007			
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<u>INDEX OF DRAWINGS</u>	<u>DWG. NO.</u>	<u>LAST REV. DATE</u>
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24I02690021 & 24I02690022  
SHELBY/ FAYETTE COUNTIES  
2026

PIN NO.: 132483.00  
DESIGN BY: SILESHI ERGICHO DATE: 12/25  
DRAWN BY: KEVIN MARTINKO DATE: 12/25  
SUPERVISED BY: DATE: 12/25  
CHECKED BY: DATE: 11/11

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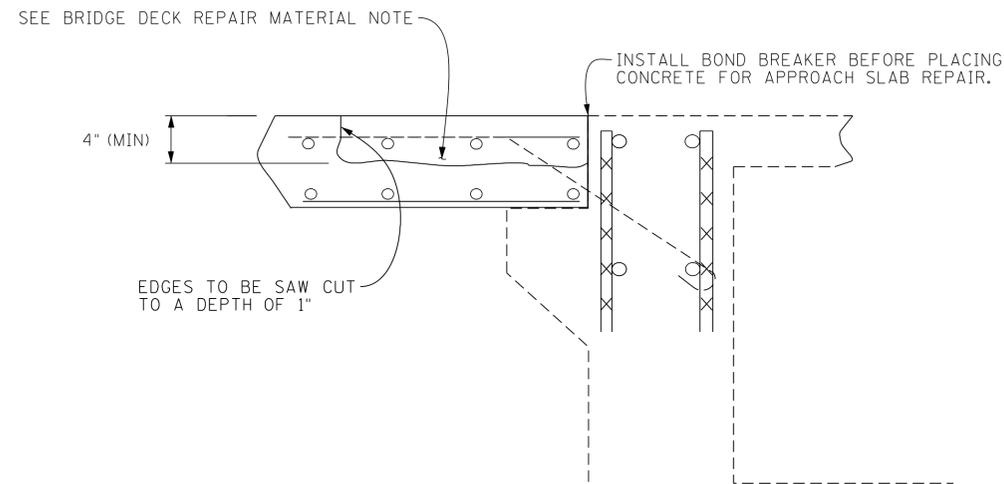
TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES					
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	PROJECT NO. 79I269-M3-007		PROJECT NO. 24I269-M3-009
			604-10.53 CONCRETE REPAIRS (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	617-04.01 TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.	617-04.01 TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
79-I269-1.32 RT. OVER WOLF RIVER OVERFLOW (79I02690009)	U-001-208 U-001-215 STD-1-5	CONCRETE REPAIRS TYPE 1 THIN EPOXY OVERLAY	5	1040	
79-I269-1.32 LT. OVER WOLF RIVER OVERFLOW (79I02690010)	U-001-208 U-001-211 STD-1-5	TYPE 1 THIN EPOXY OVERLAY		1040	
79-I269-2.13 RT. & LT. OVER OVERFOLW (79I02690015 & 79I02690016)	M-493-1 M-493-4 M-493-5 STD-1-5	TYPE 1 THIN EPOXY OVERLAY		2662	
24-I269-1.57 RT. & LT. OVER JOHNSON'S CREEK (24I02690017 & 24I02690018)	M-493-45 M-493-49 M-493-48 STD-1-5	TYPE 1 THIN EPOXY OVERLAY			1552
24-I269-2.59 RT. & LT. OVER MONTEREY ROAD (24I02690021 & 24I02690022)	M-493-64 M-493-67 M-493-68 STD-1-5	TYPE 1 THIN EPOXY OVERLAY			2144
TOTAL			5	4742	3696



**STATE OF TENNESSEE**  
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 BRIDGE TABULATION &  
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 79-I269-1.32 RT. & LT. OVER  
 WOLF RIVER OVERFLOW,  
 79-I269-2.13 RT. & LT. OVER  
 OVERFLOW,  
 24-I269-1.57 RT. & LT. OVER  
 JOHNSON'S CREEK AND  
 24-I269-2.59 RT. & LT. OVER  
 MONTEREY ROAD  
 FED. BRIDGE ID NOS.  
 79I02690009, 79I02690010,  
 79I02690015, 79I02690016  
 24I02690017, 24I02690018,  
 24I02690021 & 24I02690022  
 SHELBY/ FAYETTE COUNTIES  
 2026

PIN NO.: 132843.00  
 DESIGN BY: SILESHI ERGICHO DATE: 11/25  
 DRAWN BY: SILESHI ERGICHO DATE: 12/25  
 SUPERVISED BY: KEVIN MARTINKO DATE: 12/25  
 CHECKED BY: DATE: 11/25

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79I269-M3-007	2026	B-3	
REVISIONS			
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**CONCRETE APPROACH PAVEMENT REPAIR DETAILS:**

- NOTE: REMOVE CONCRETE TO A DEPTH OF 3/4" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".
- NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR S.Y.
- NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- NOTE: CONCRETE REPAIRS AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF CONCRETE REPAIRS SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

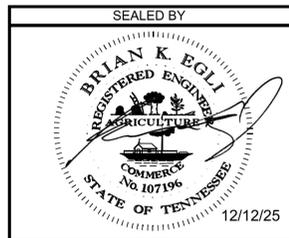
**BRIDGE DECK REPAIR MATERIAL**

ALL REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHET QUICK SET PATCHING MATERIAL FROM THE TDOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIUOS PATCHING MATERIALS. PATCHING MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS. MIX MUST MEET 3000 P.S.I. BEFORE OPENING TO TRAFFIC.

**POWER DRIVEN HAND TOOLS:**

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

- (1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. ALSO TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.
- (2) FULL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 LB. CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 LB. PNEUMATIC HAMMERS.
- (3) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

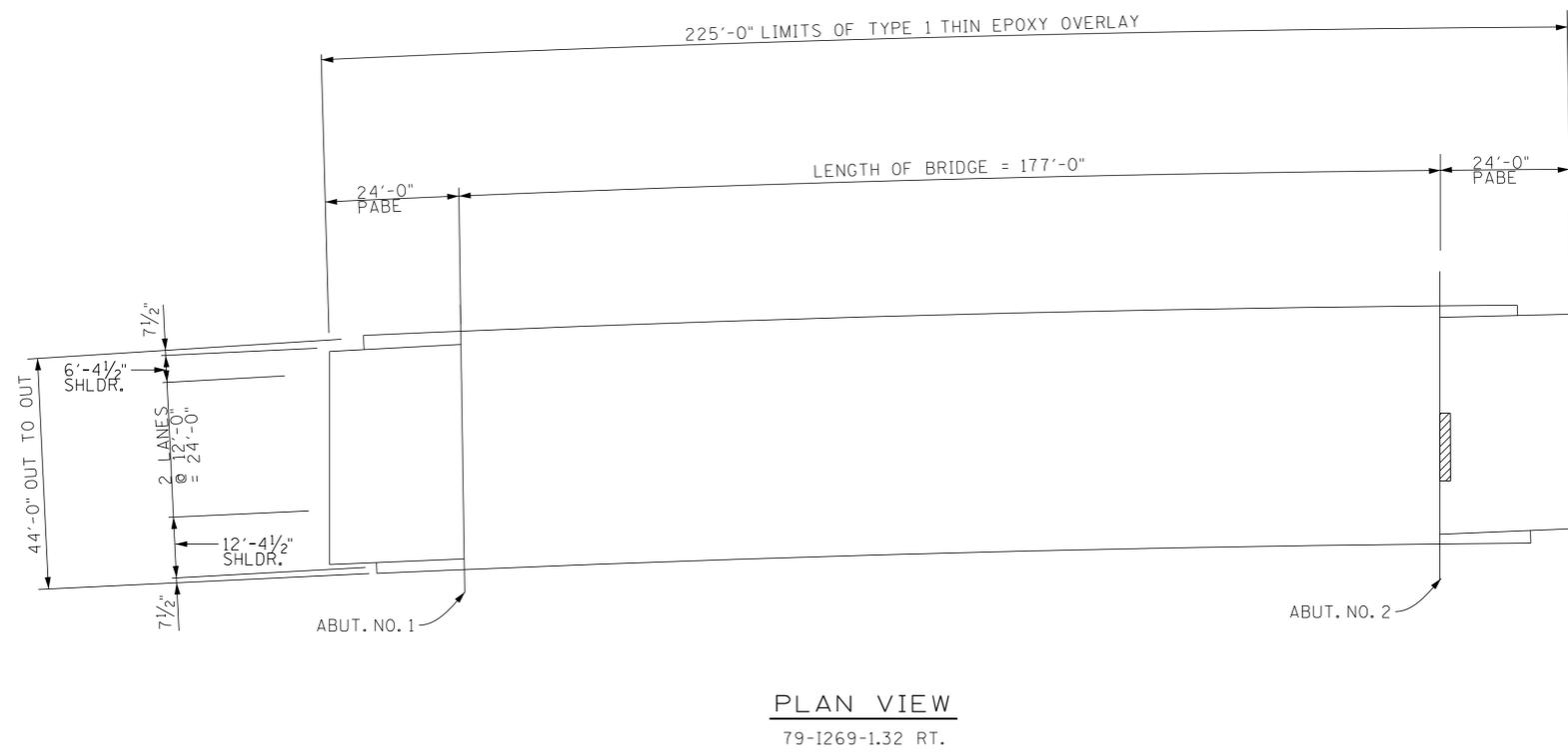
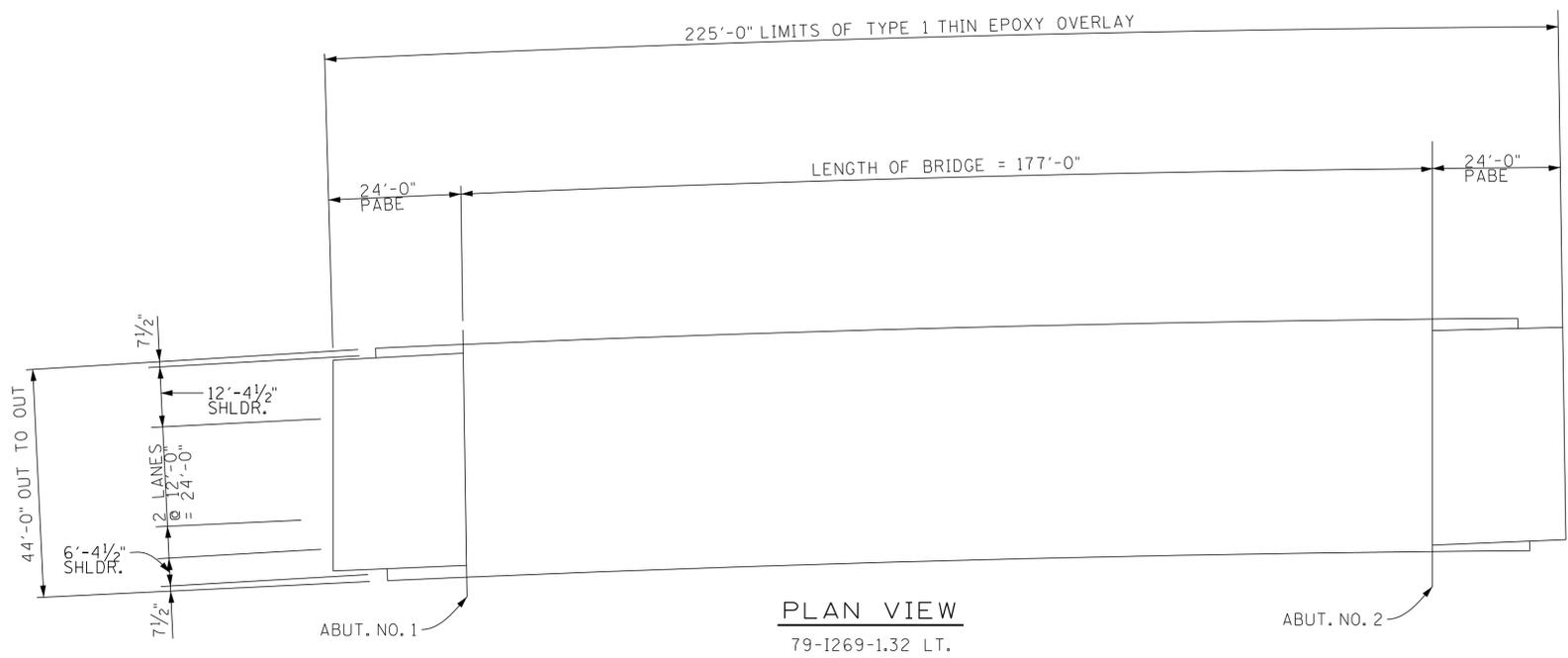


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
REPAIRS NOTES AND DETAILS  
79-I269-1.32 RT.  
OVER WOLF RIVER  
FED. BRIDGE ID NO.  
79I02690009  
SHELBY COUNTY  
2026

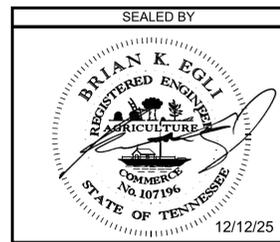
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DESIGN BY: SILESHI ERGICHO DATE: 12/25  
DRAWN BY: KEVIN MARTINKO DATE: 12/25  
SUPERVISED BY: DATE: 12/25  
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PROJECT NO.	YEAR	SHEET NO.
79I269-M3-007	2026	B-4

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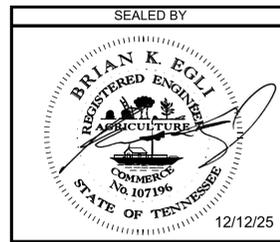
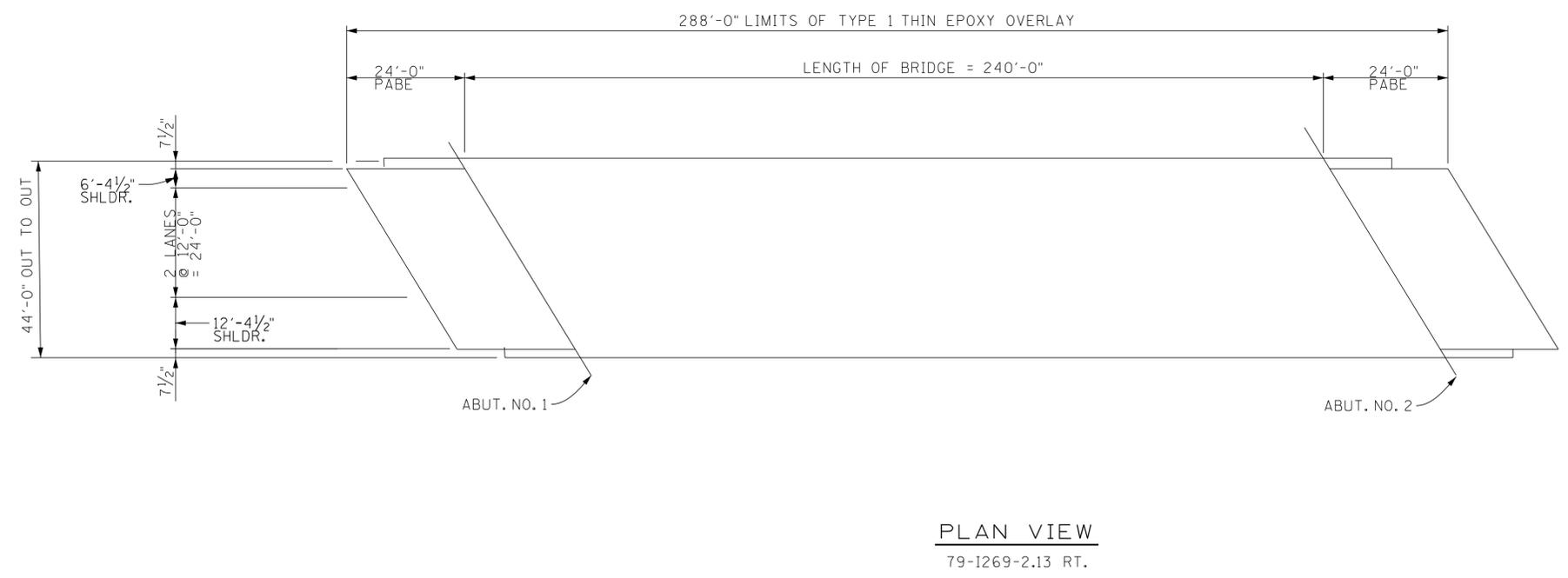
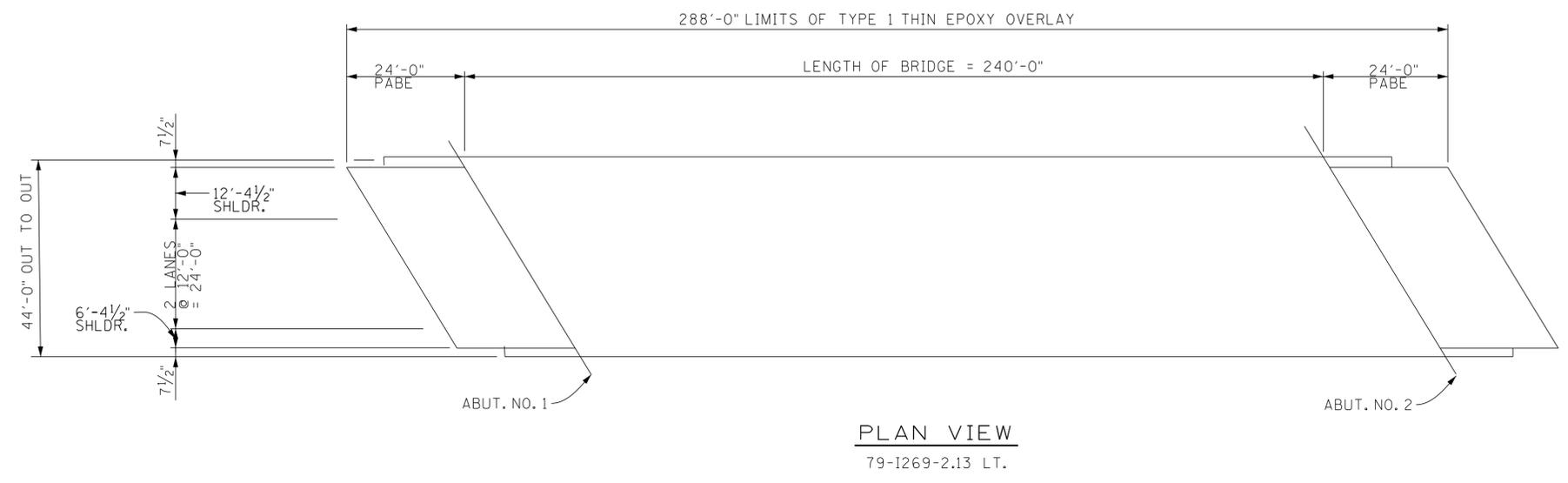
DENOTES: APPROXIMATE CONCRETE REPAIRS LOCATION (PARTIAL DEPTH OF APPROACH PAVEMENT)



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW APPROXIMATE  
REPAIRS LOCATIONS  
79-I269-1.32 RT. & LT. OVER  
WOLF RIVER OVERFLOW  
FED. BRIDGE ID NOS.  
79I02690009 & 79I02690010  
SHELBY COUNTY  
2026

PIN NO.: 132843.00  
DESIGN BY: SILESHI ERGICHO DATE: 12/25  
DRAWN BY: KEVIN MARTINKO DATE: 12/25  
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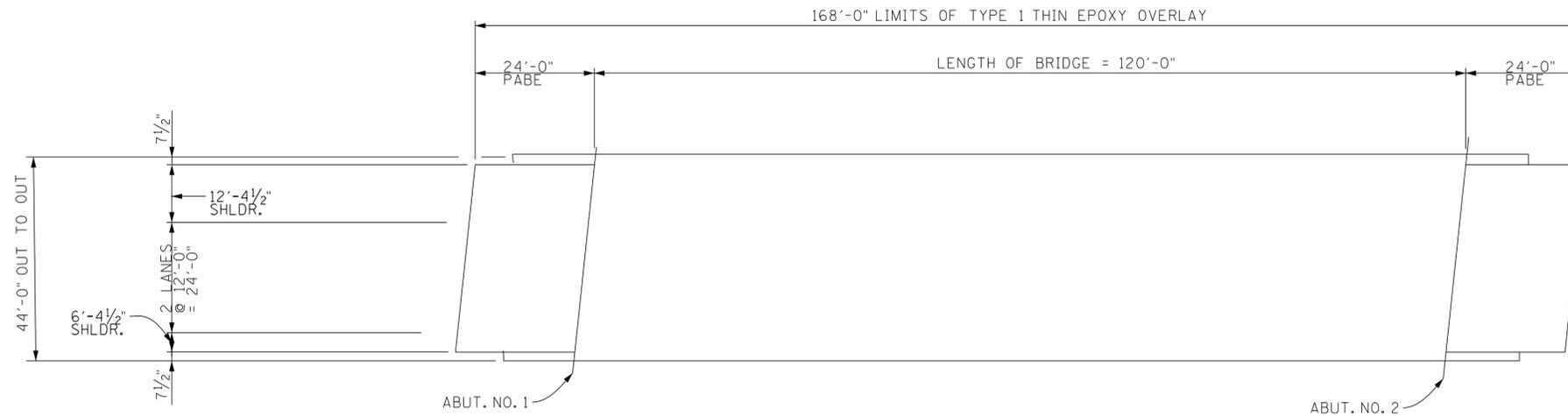
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79I269-M3-007	2026	B-5	
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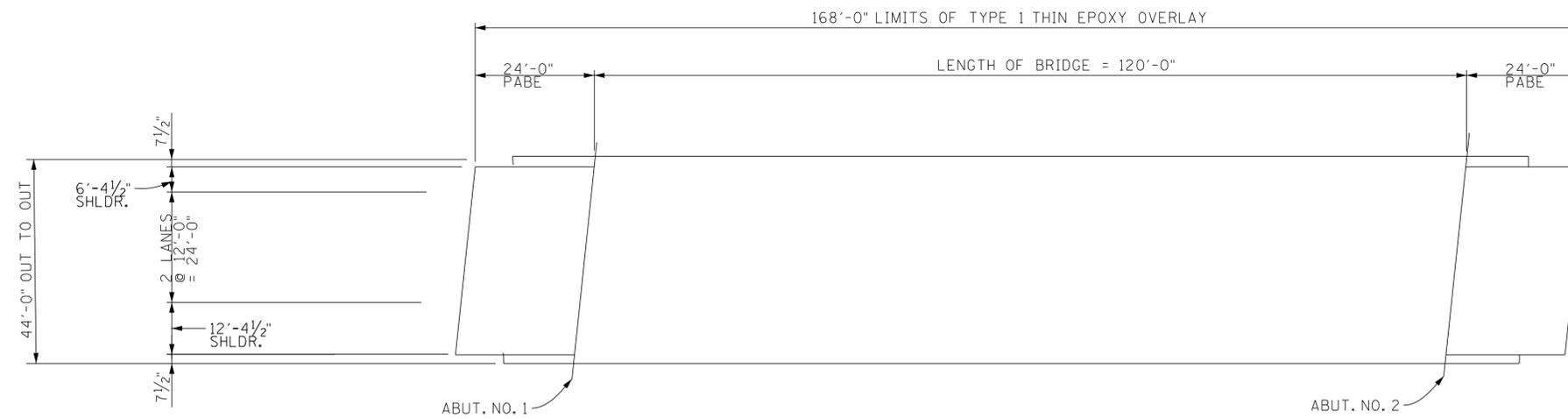
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DEPARTMENT OF TRANSPORTATION  
PLAN VIEW APPROXIMATE  
REPAIRS LOCATIONS  
79-I269-2.13 RT. & LT. OVER  
OVERFLOW  
FED. BRIDGE ID NOS.  
79I02690015 & 79I02690016  
SHELBY COUNTY  
2026

PIN NO.: 132843.00  
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SUPERVISED BY: KEVIN MARTINKO DATE: 12/25  
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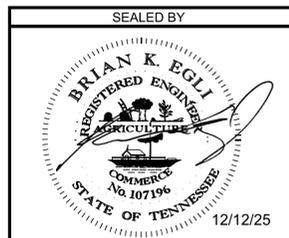
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PLAN VIEW  
24-I269-1.57 LT.



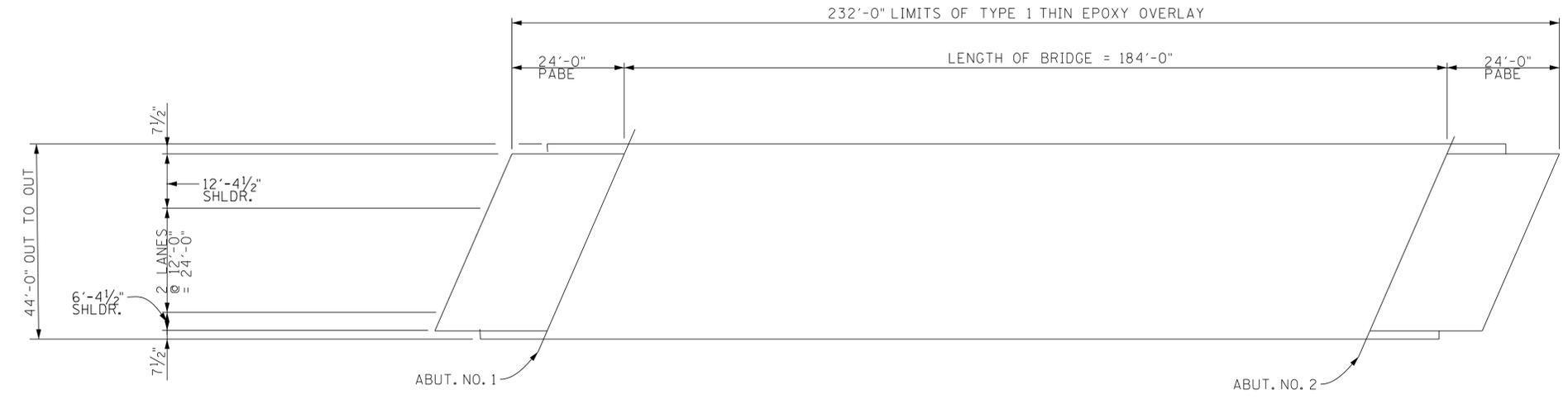
PLAN VIEW  
24-I269-1.57 RT.



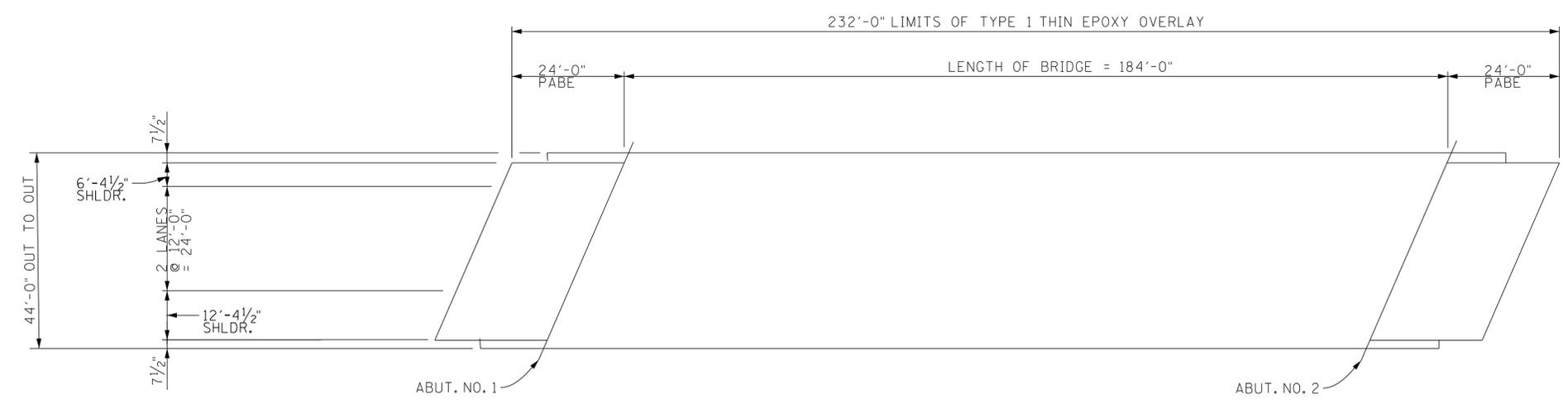
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW APPROXIMATE  
REPAIRS LOCATIONS  
24-I269-1.57 RT. & LT. OVER  
JOHNSON'S CREEK  
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PIN NO.: 132843.00  
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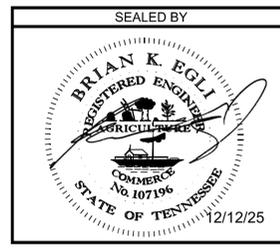
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PLAN VIEW  
24-I269-2.59 LT.



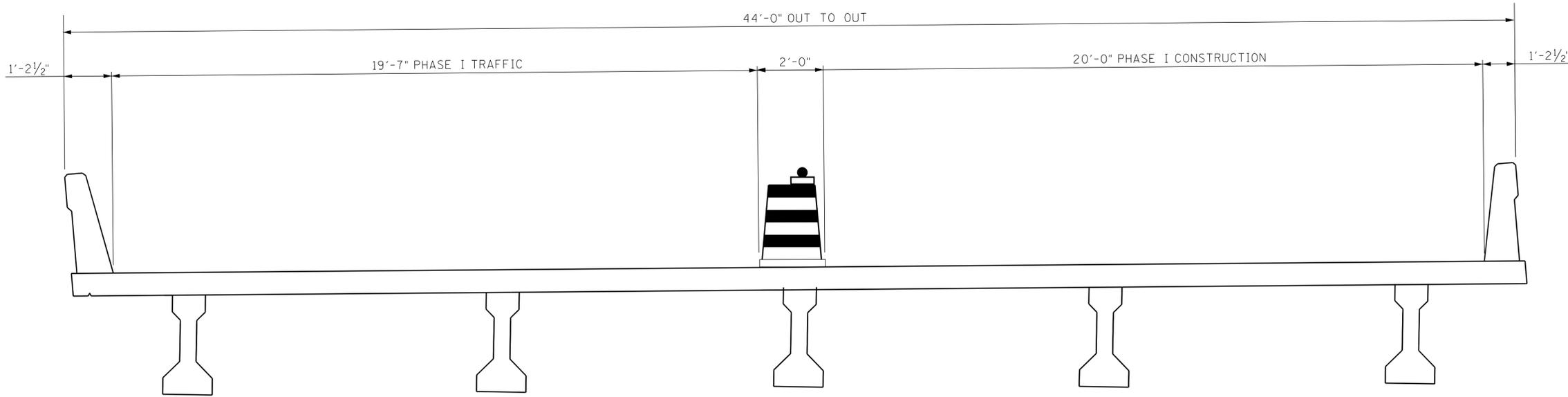
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24-I269-2.59 RT.



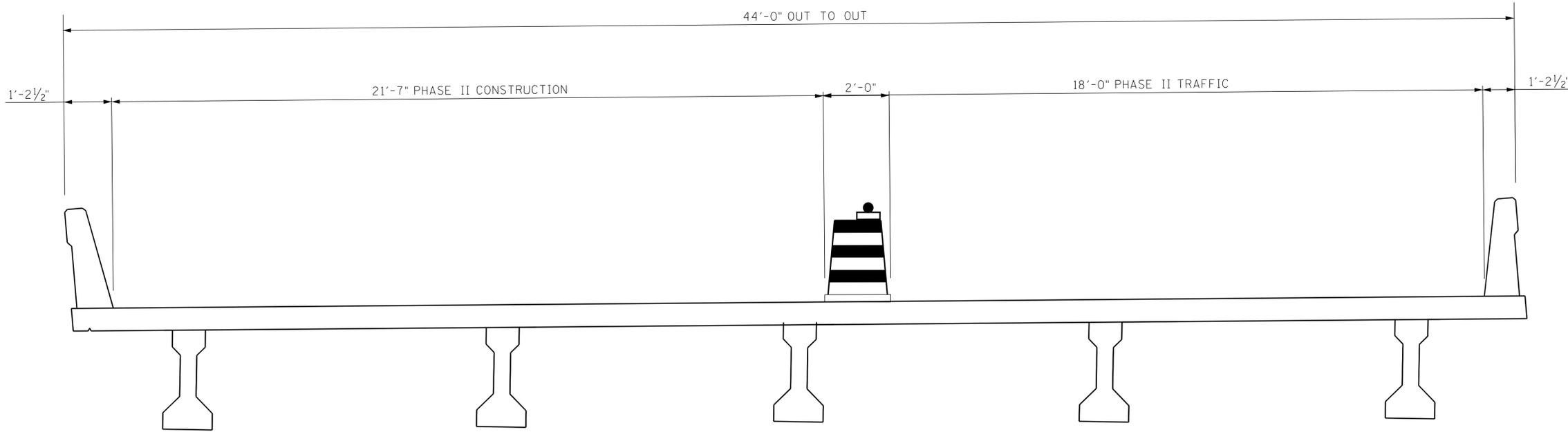
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW APPROXIMATE  
REPAIRS LOCATIONS  
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MONTEREY ROAD  
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PIN NO.: 132843.00  
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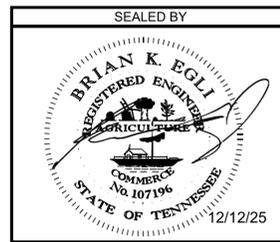
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79I269-M3-007			
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**PHASE I CONSTRUCTION**  
 (LEFT LANE - LOOKING BACK ON THE SURVEY)  
 (RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



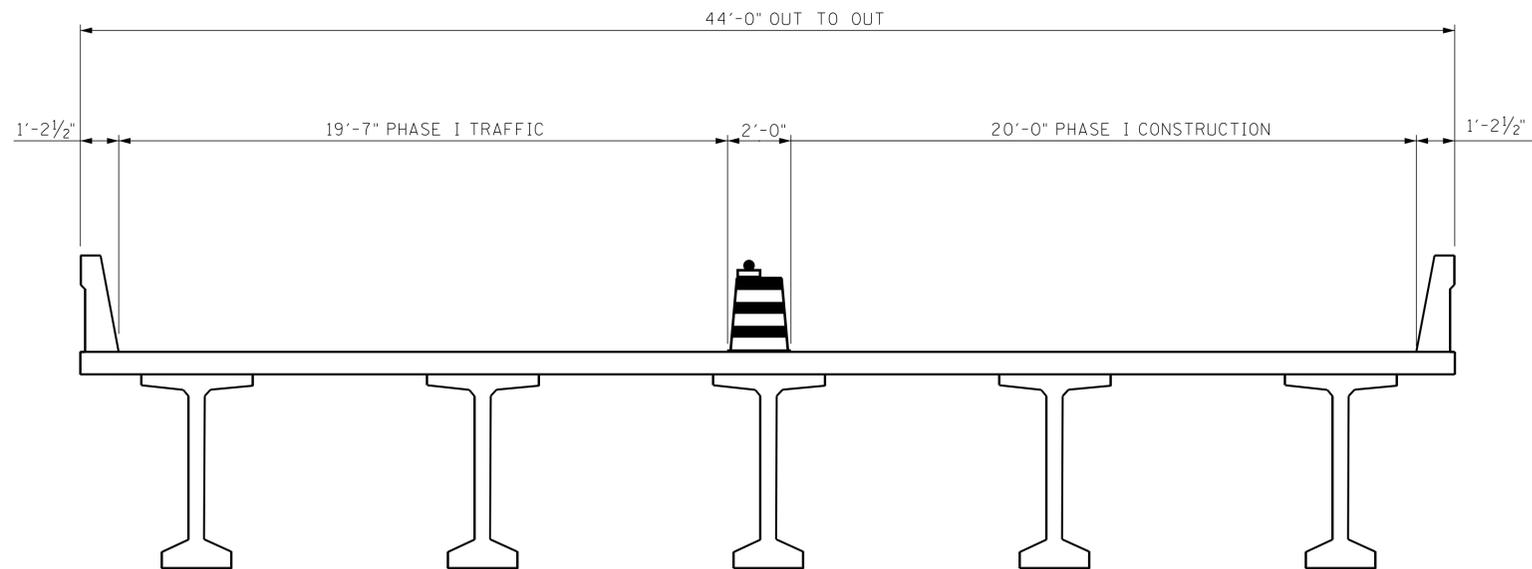
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 (RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



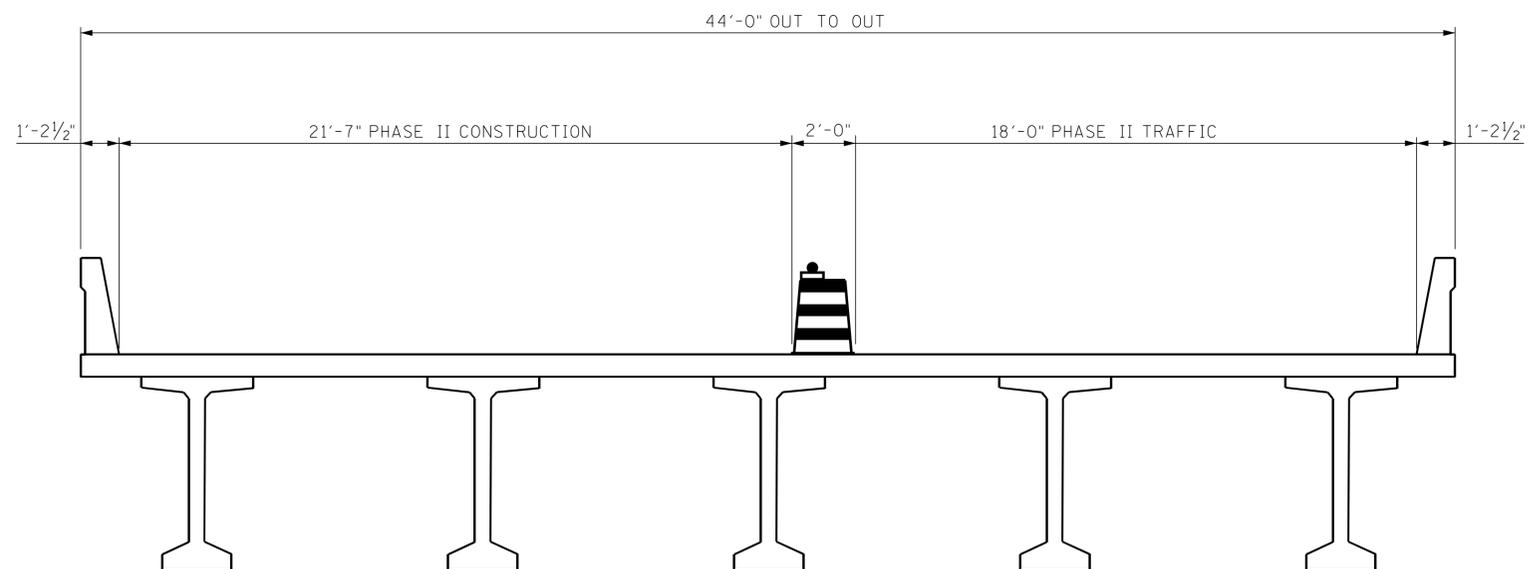
STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 PHASE CONSTRUCTION  
 79-I269-1.32 RT. & LT. OVER  
 WOLF RIVER OVERFLOW &  
 24-I269-2.59 RT. & LT. OVER  
 MONTEREY ROAD  
 FED. BRIDGE ID NOS.  
 79I02690009, 79I02690010  
 24I02690021 & 24I02690022  
 SHELBY / FAYETTE COUNTIES  
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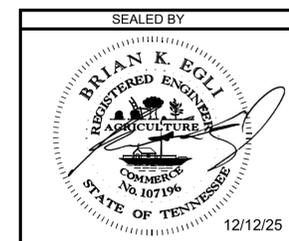
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791269-M3-007			
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**PHASE I CONSTRUCTION**  
 (LEFT LANE - LOOKING BACK ON THE SURVEY)  
 (RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



**PHASE II CONSTRUCTION**  
 (LEFT LANE - LOOKING BACK ON THE SURVEY)  
 (RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



STATE OF TENNESSEE  
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PHASE CONSTRUCTION  
 79-1269-2.13 RT. & LT. OVER  
 OVERFLOW AND  
 24-1269-1.57 RT. & LT. OVER  
 JOHNSON'S CREEK  
 FED. BRIDGE ID NOS.  
 79102690015, 79102690016,  
 24102690017 & 24102690018  
 SHELBY / FAYETTE COUNTIES  
 2026

PIN NO.: 132843.00

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79I269-M3-007			
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**TYPE 1 THIN EPOXY OVERLAY NOTES :**

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

- A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.
- B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.
- C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.
- D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.
- E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.
- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO THE APPLICATION OF THE THIN OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

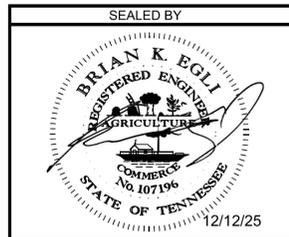
THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), S.Y.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES MADE BY THE CONTRACTOR WITH A CORING BIT NOT LESS THAN 1 1/2" DIAMETER. THE TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

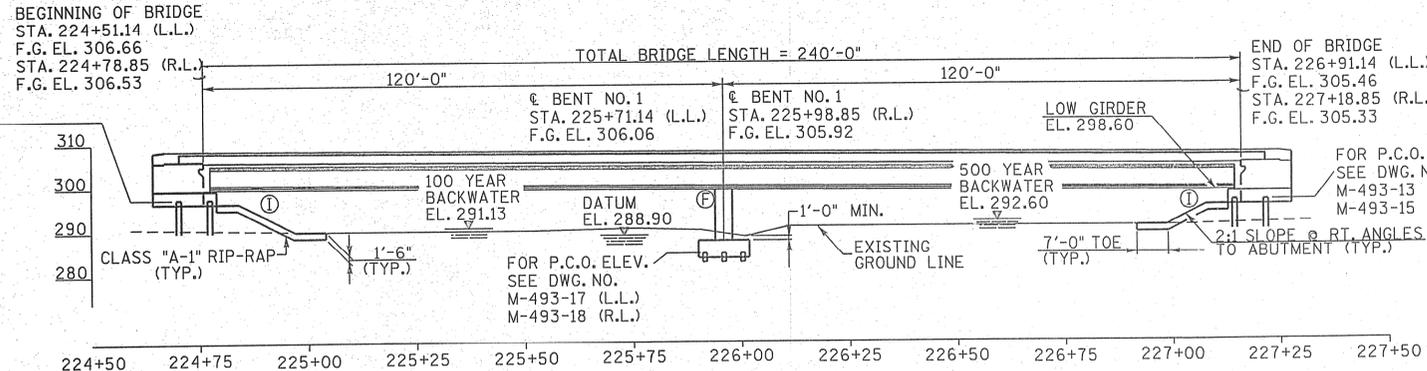
THE MANUFACTURER SHALL HAVE A REP ON THE JOB SITE AT ALL TIME DURING APPLICATION AND CURE TIME. THE REP WITH THE ENGINEER, MAY SUSPEND AN ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REP ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

**\*\* SPECIAL NOTE:**  
 THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACKCOAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY.  
 MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACKCOAT OR DEBRIS REMOVAL.  
 REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.



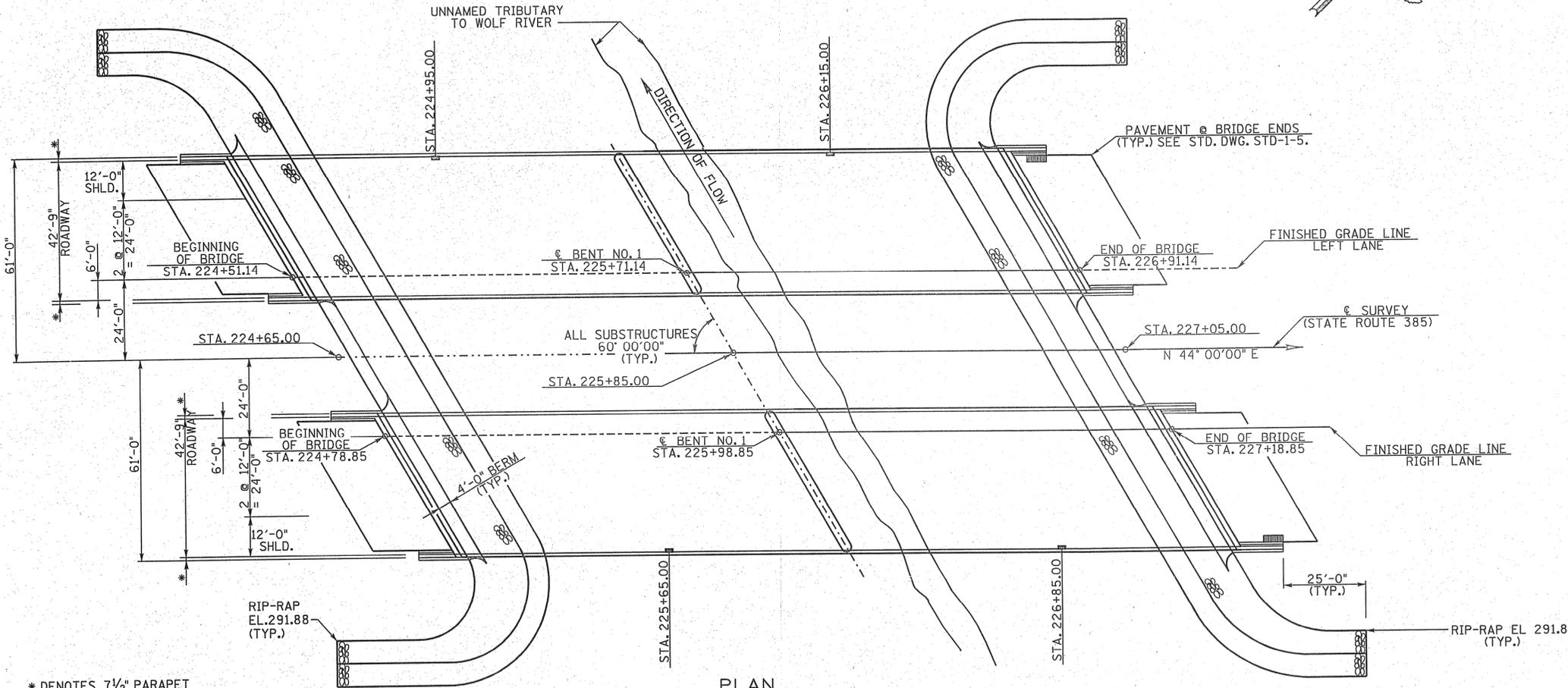
STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 TYPE 1 THIN EPOXY  
 OVERLAY NOTES  
 79-I269-1.32 RT. & LT. OVER  
 WOLF RIVER OVERFLOW,  
 79-I269-2.13 RT. & LT. OVER  
 OVERFLOW,  
 24-I269-1.57 RT. & LT. OVER  
 JOHNSON'S CREEK AND  
 24-I269-2.59 RT. & LT. OVER  
 MONTEREY ROAD  
 FED. BRIDGE ID NOS.  
 79I02690009, 79I02690010,  
 79I02690015, 79I02690016  
 24I02690017, 24I02690018,  
 24I02690021 & 24I02690022  
 SHELBY/ FAYETTE COUNTIES  
 2026

PIN NO.: 132843.00  
 DESIGN BY: SILESHI ERGICHO DATE: 12/25  
 DRAWN BY: KEVIN MARTINKO DATE: 12/25  
 SUPERVISED BY: DATE: 12/25  
 CHECKED BY: DATE: 11



ELEVATION  
SCALE: 1" = 20'

⊕ DENOTES : FIXED  
⊙ DENOTES : INTERGRAL



PLAN  
SCALE: 1" = 20'

\* DENOTES 7 1/2" PARAPET  
■ DENOTES PARAPET DRAIN  
▨ DENOTES END OF BRIDGE DRAIN (2'-0" X 8'-7") (STD-1-6,7&8)

HYDRAULIC DATA

DRAINAGE AREA = 604.7 sq. mi.  
DESIGN DISCHARGE (100 YR.) = 315 cfs  
TOTAL DESIGN DISCHARGE = 31,356 cfs  
WATER AREA PROVIDED BELOW EL. 291.56 = 554.0 sq.ft.  
100 YEAR VELOCITY = 1.22 ft/sec  
100 YEAR BRIDGE BACKWATER = 0.09 FT. AT EL. 291.13  
ROADWAY OVERTOPPING EL. = 295.00  
500 YEAR DISCHARGE = 47,494 cfs AT EL. 292.60.

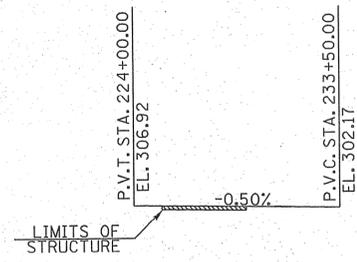
LIST OF SPECIAL PROVISIONS PROV. NO. REV. DATE

FOUNDATION PREPARATION ..... 604F.....

NOTE: BRIDGE NOS. ARE OUT OF SEQUENCE DUE TO CHANGES IN SECTION BREAK POINTS OVER THE COURSE OF PROJECT DEVELOPMENT.

NOTE: ANY WORK WITHIN THE STREAM CHANNEL AREA (E.G., FOR PIER FOOTING, RIP-RAP PLACEMENT, MULTI-BARREL CULVERT/ BRIDGE CONSTRUCTION, ETC.) SHALL BE SEPARATED FROM FLOWING WATER OR EXPECTED FLOW PATH AND PERFORMED DURING LOW FLOW CONDITIONS. ALL ITEMS USED WITHIN THE STREAM CHANNEL AREA FOR DIVERSION OF FLOW (OR EXPECTED FLOW), UNLESS SPECIFIED IN THE PLANS, SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE COST OF OTHER ITEMS. THIS NOTE EXCLUDES ANY ITEMS SPECIFIED IN THE PLANS FOR THE TEMPORARY DIVERSION CHANNELS, EC-STR-31 AND TEMPORARY DIVERSION CULVERTS, EC-STR-32 FOR SINGLE BARREL CULVERT CONSTRUCTION.

DESIGNED BY M. BARRELL DATE 9-01  
DRAWN BY FARRAR/MOSHER/DE DATE 11-01  
SUPERVISED BY FIELDS/WOODS DATE 11-01  
CHECKED BY M. BARRELL DATE 1-04



GRADE SKETCH  
(STATE ROUTE 385)

LIST OF DRAWINGS

DWG. NO.	REV. DATE
LAYOUT OF BRIDGE	M-493-1
GENERAL NOTES & ESTIMATED QUANTITIES	M-493-2
FOUNDATION DATA	M-493-3
SUPERSTRUCTURE (LEFT LANE)	M-493-4
SUPERSTRUCTURE (RIGHT LANE)	M-493-5
SUPERSTRUCTURE DETAILS (LEFT & RIGHT LANES)	M-493-6
SUPERSTRUCTURE DETAILS (LEFT & RIGHT LANES)	M-493-7
PRESTRESSED BULB-TEE BEAMS, SPANS 1 & 2 (LT. & RT. LNS)	M-493-8
ABUTMENT NO. 1 (LEFT LANE)	M-493-9
ABUTMENT NO. 1 DETAILS (LEFT LANE)	M-493-10
ABUTMENT NO. 1 (RIGHT LANE)	M-493-11
ABUTMENT NO. 1 DETAILS (RIGHT LANE)	M-493-12
ABUTMENT NO. 2 (LEFT LANE)	M-493-13
ABUTMENT NO. 2 DETAILS (LEFT LANE)	M-493-14
ABUTMENT NO. 2 (RIGHT LANE)	M-493-15
ABUTMENT NO. 2 DETAILS (RIGHT LANE)	M-493-16
BENT NO. 1 (LEFT LANE)	M-493-17
BENT NO. 1 (RIGHT LANE)	M-493-18
BENT NO. 1 DETAILS (LEFT & RIGHT LANES)	M-493-19
FINAL FOUNDATION DATA	M-493-20
BILL OF STEEL (LEFT LANE)	M-493-21
BILL OF STEEL (LEFT LANE)	M-493-22
BILL OF STEEL (RIGHT LANE)	M-493-23
BILL OF STEEL (RIGHT LANE)	M-493-24

LIST OF STANDARD DRAWINGS DWG. NO. REV. DATE

BRIDGE RAILING SINGLE SLOPE CONCRETE PARAPET	STD-1-1SS	10-15-08
SLIDER PLATES AND DECK DRAINS	STD-1-2	3-28-08
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-5	8-08-08
BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-6	4-28-97
BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-7	7-31-00
BRIDGE END DRAIN DETAILS 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-9	5-1-95
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-1	4-8-05
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-2	4-8-05
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-3	3-02-02
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STD-4-4	6-10-96
STANDARD PILE DETAILS	STD-5-1	10-25-93
STANDARD PILE DETAILS	STD-5-2	4-8-05
STANDARD SEISMIC DETAILS	STD-6-1	5-21-99
STANDARD SEISMIC DETAILS	STD-6-2	11-07-94
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STD-10-1	4-8-05
STD. DETAILS AND INT. DIAPH. DETAILS FOR BULB-TEE BEAMS	STD-14-1	10-15-08

2022 ADT = 30,670  
DUAL - 42'-9" ROADWAYS STD-1-1SS BRIDGERAIL  
DESIGN SPEED = 60 MPH

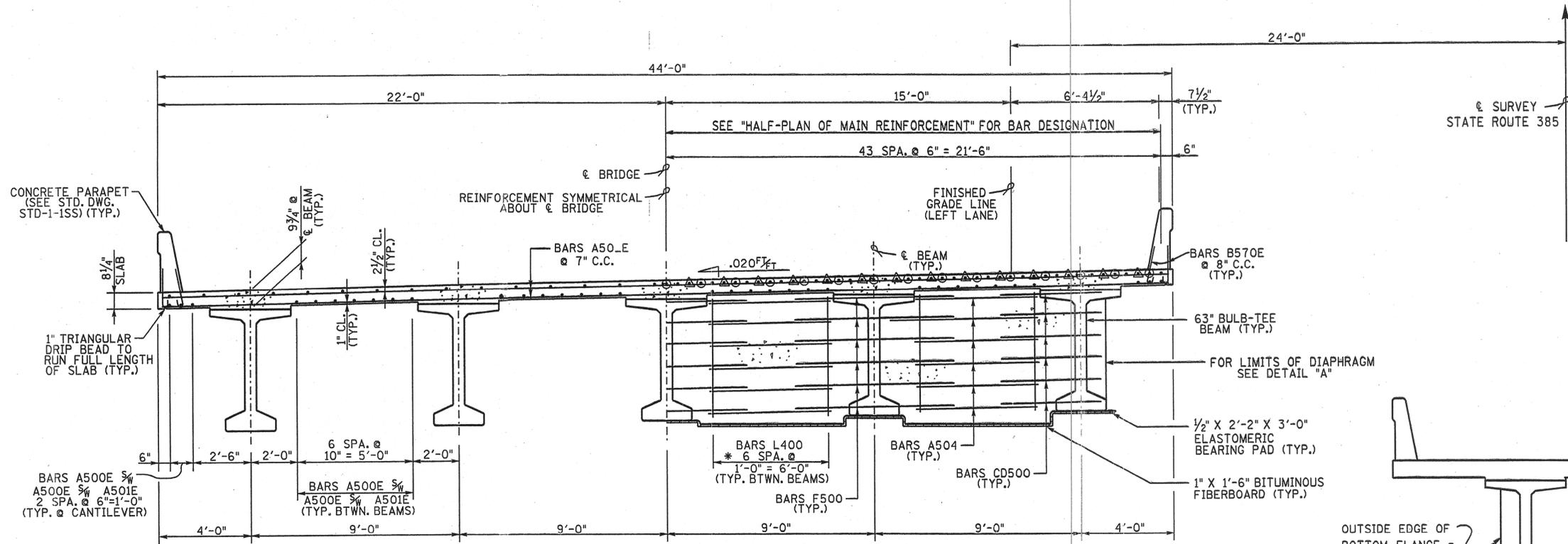
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 5  
LAYOUT OF BRIDGE  
STATE ROUTE 385  
OVER  
UNNAMED TRIBUTARY TO WOLF RIVER  
BRIDGE I.D. NO. 79SR3850103 (RIGHT LANE)  
BRIDGE I.D. NO. 79SR3850104 (LEFT LANE)  
STATION 225+85.00  
SHELBY-FAYETTE COUNTIES  
2008

CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

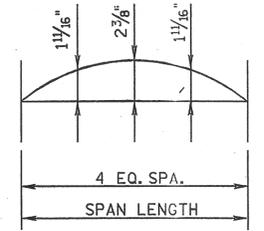
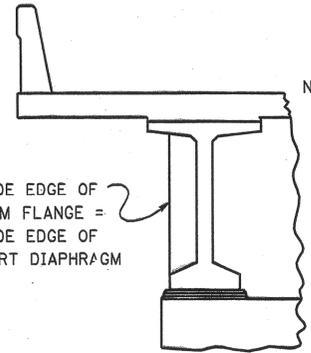


NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO, SEE STANDARD DRAWING STD-1-1SS.

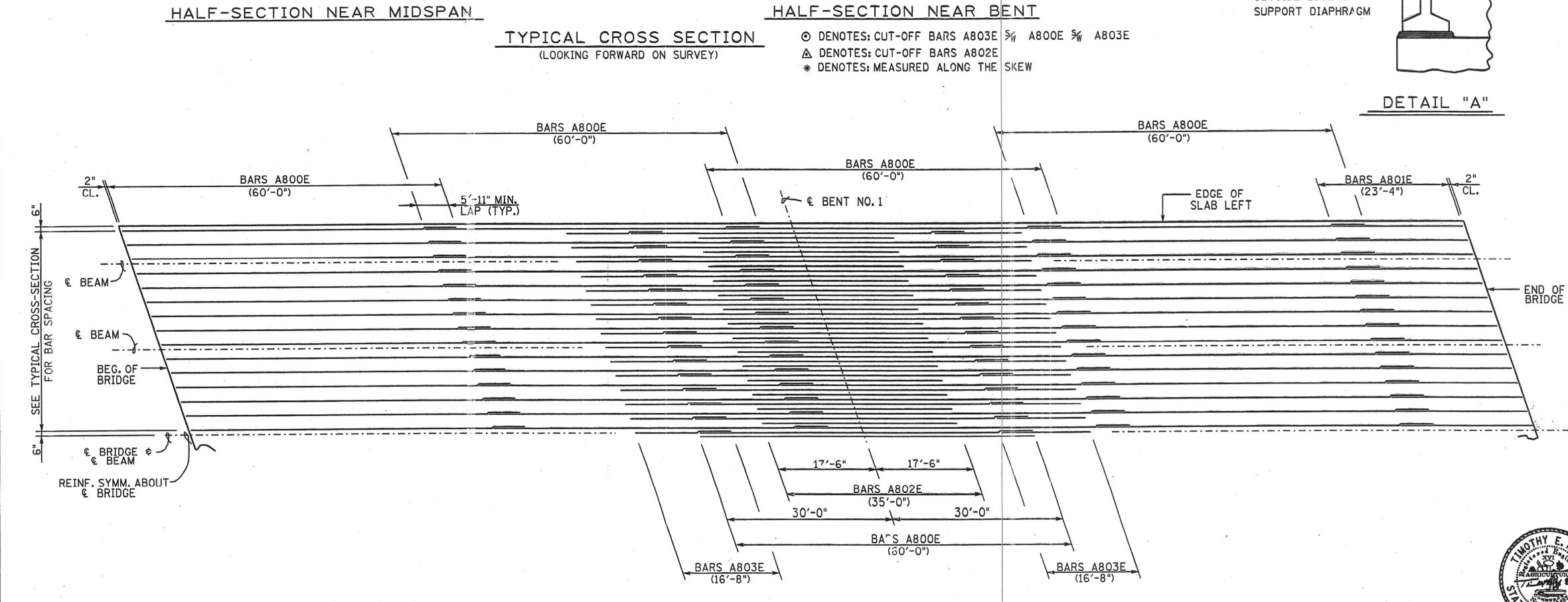
NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

NOTE: THE SUPPORT DIAPHRAGMS AT THE BENTS SHALL BE FORMED AND THE BOTTOM 15 INCHES POURED AS SOON AS POSSIBLE AFTER THE BEAMS HAVE BEEN SET. THE REMAINDER OF THE DIAPHRAGM SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB. ALL DIAPHRAGMS CONCRETE SHALL BE INCLUDED IN THE QUANTITY FOR ITEM 604-03.09.



**DEAD LOAD CORRECTION CURVE**  
 THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE.

IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.



ESTIMATED QUANTITIES			
LOCATION	CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.	STEEL BAR REINFORCEMENT LB.
LEFT LANE	297	80,290	1021



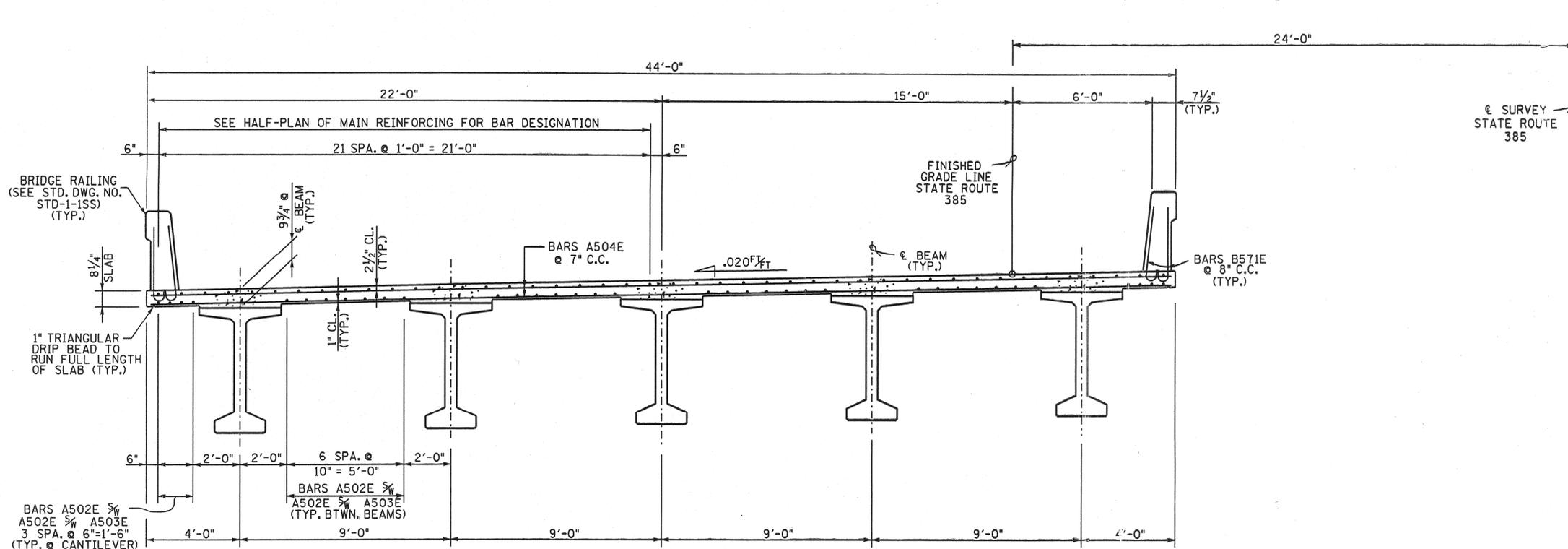
DESIGNED BY M.BARRELL DATE 9-01  
 DRAWN BY J.MAYER / S.NELSON DATE 10-01  
 SUPERVISED BY FIELDS / WOODS DATE 10-01  
 CHECKED BY M.BARRELL DATE 1-04

CONTRACT Edward P. Wasserman  
 ENGINEER OF STRUCTURES

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BRIDGE NO. 5  
 SUPERSTRUCTURE  
 STATE ROUTE 385  
 (LEFT LANE)  
 OVER UNNAMED TRIBUTARY  
 TO WOLF RIVER  
 STATION 225+85.00  
 SHELBY/FAYETTE COUNTIES  
 2008







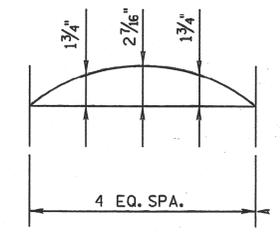
TYPICAL CROSS SECTION  
(LOOKING FORWARD ON SURVEY)

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE BRIDGE RAIL SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO, SEE STANDARD DRAWING STD-1-1SS.

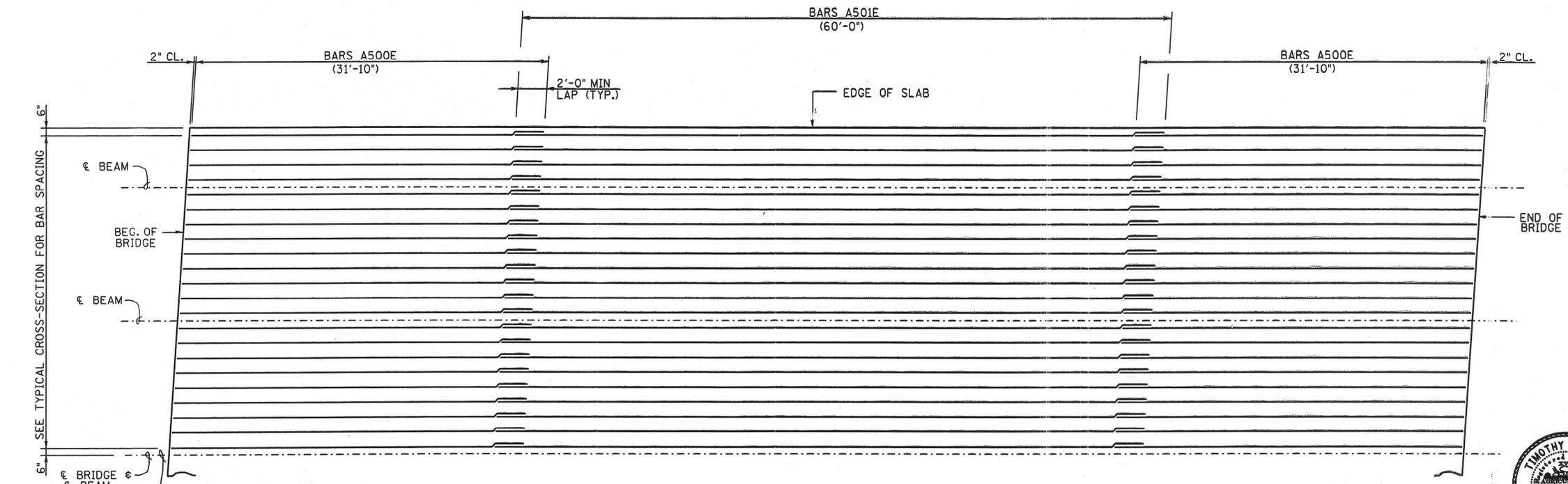
NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED PRIOR TO PLACING ANY LOADS ON THE GIRDERS. HOWEVER, TEMPORARY ERECTION DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.



DEAD LOAD CORRECTION CURVE

THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE.

IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.



HALF-PLAN OF MAIN REINFORCEMENT

ESTIMATED QUANTITIES		
LOCATION	CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.
LEFT LANE	141	31075



STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BRIDGE NO. 7  
 SUPERSTRUCTURE  
 STATE ROUTE 385  
 (LEFT LANE)  
 OVER JOHNSON'S CREEK  
 STATION 310+12.00  
 SHELBY/FAYETTE COUNTIES  
 2009

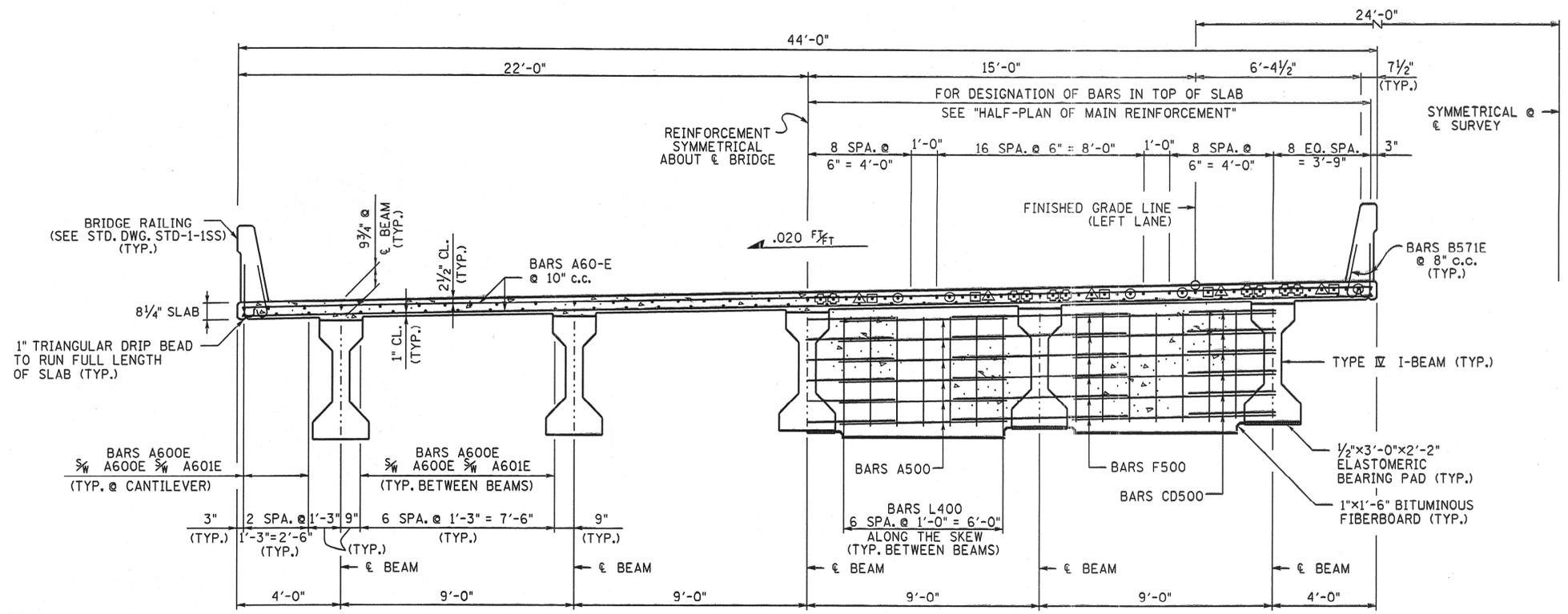
CORRECT Edward P. Wasserman  
 ENGINEER OF STRUCTURES

DESIGNED BY A. PRICE DATE 9-01  
 DRAWN BY J. MAYER / D.E. DATE 10-01  
 SUPERVISED BY FIELDS / WOODS DATE 10-01  
 CHECKED BY A. PRICE DATE 3-03





CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

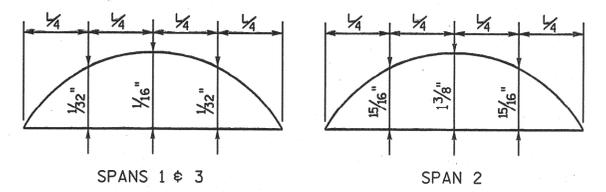


NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE DRAWING NO. STD-1-1SS.

NOTE: SUPPORT DIAPHRAGMS AT BENTS SHALL BE FORMED AND THE BOTTOM 15 INCHES POURED AS SOON AS POSSIBLE AFTER THE BEAMS HAVE BEEN SET. THE REMAINDER OF THE DIAPHRAGM SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB. ALL DIAPHRAGM CONCRETE SHALL BE INCLUDED IN THE QUANTITY FOR ITEM 604-03.09.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.



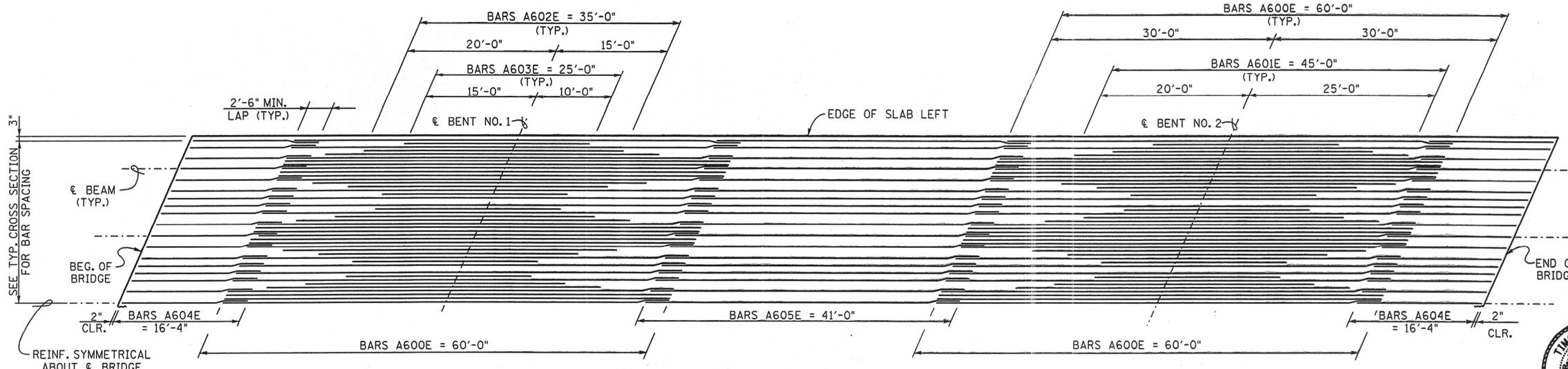
**DEAD LOAD CORRECTION CURVE**

THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.

IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

⊗ DENOTES: CUT-OFF BARS A600E  
 △ DENOTES: CUT-OFF BARS A601E  
 □ DENOTES: CUT-OFF BARS A602E  
 ○ DENOTES: CUT-OFF BARS A603E

HALF-SECTION NEAR MIDSPAN      HALF-SECTION NEAR BENT  
**TYPICAL CROSS SECTION**  
 (LOOKING FORWARD ON SURVEY)



**HALF-PLAN OF MAIN REINFORCEMENT**

ESTIMATED QUANTITIES		
CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.	REINFORCING STEEL LB.
232	58,911	1,954

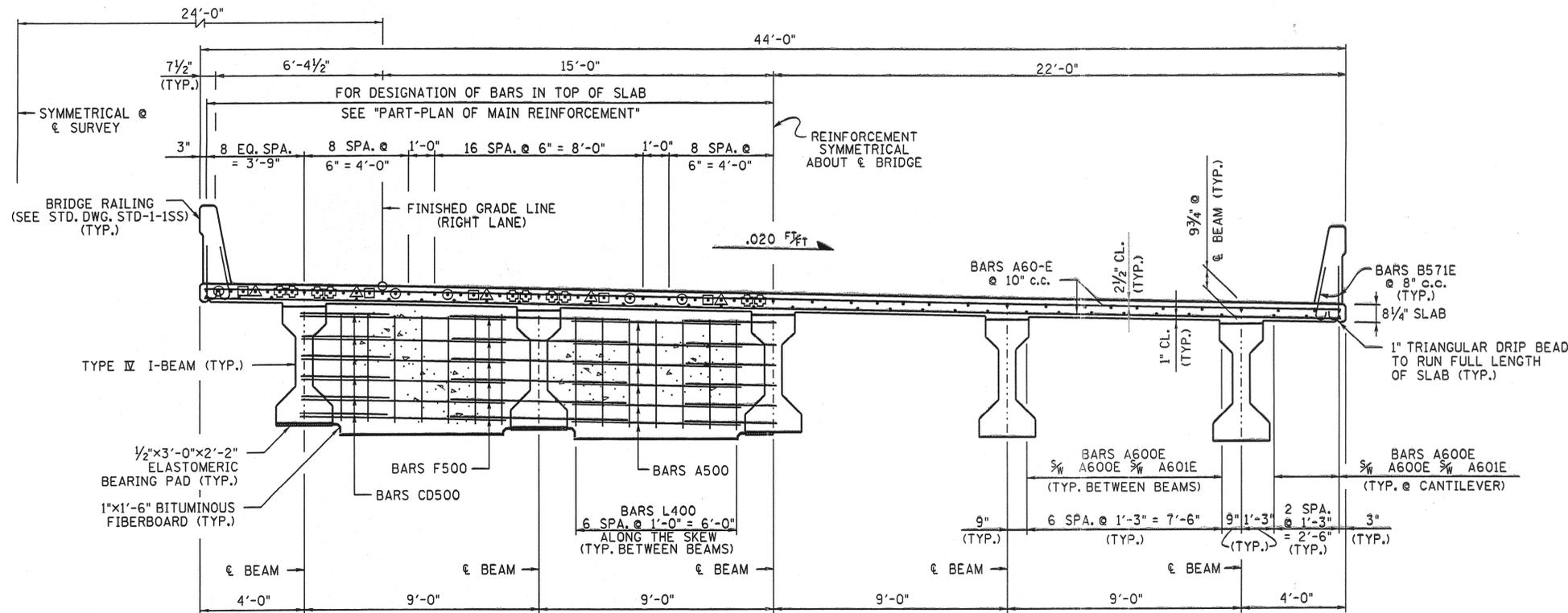


STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
**BRIDGE NO. 8**  
 SUPERSTRUCTURE  
 STATE ROUTE 385  
 (LEFT LANE)  
 OVER  
 MONTEREY ROAD  
 STATION 364+26.76  
 SHELBY-FAYETTE COUNTIES  
 2009

CORRECT *Edward P. Wasserman*  
 ENGINEER OF STRUCTURES

DESIGNED BY K. McALISTER      DATE 10-01  
 DRAWN BY F. BARTON              DATE 8-02  
 SUPERVISED BY FIELDS & WOODS      DATE 8-02  
 CHECKED BY \_\_\_\_\_              DATE \_\_\_\_\_

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

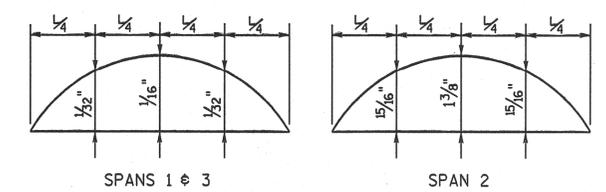


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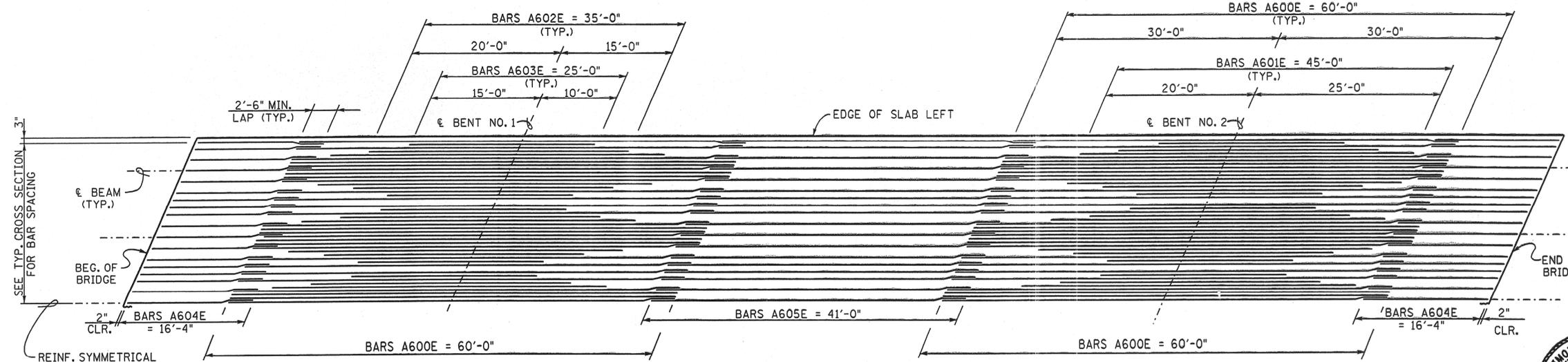
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THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.

IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

- ⊗ DENOTES: CUT-OFF BARS A600E
- ⊠ DENOTES: CUT-OFF BARS A601E
- ⊡ DENOTES: CUT-OFF BARS A602E
- ⊢ DENOTES: CUT-OFF BARS A603E

TYPICAL CROSS SECTION (LOOKING FORWARD ON SURVEY)



HALF-PLAN OF MAIN REINFORCEMENT

ESTIMATED QUANTITIES		
CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.	REINFORCING STEEL LB.
232	58,911	1,954

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION

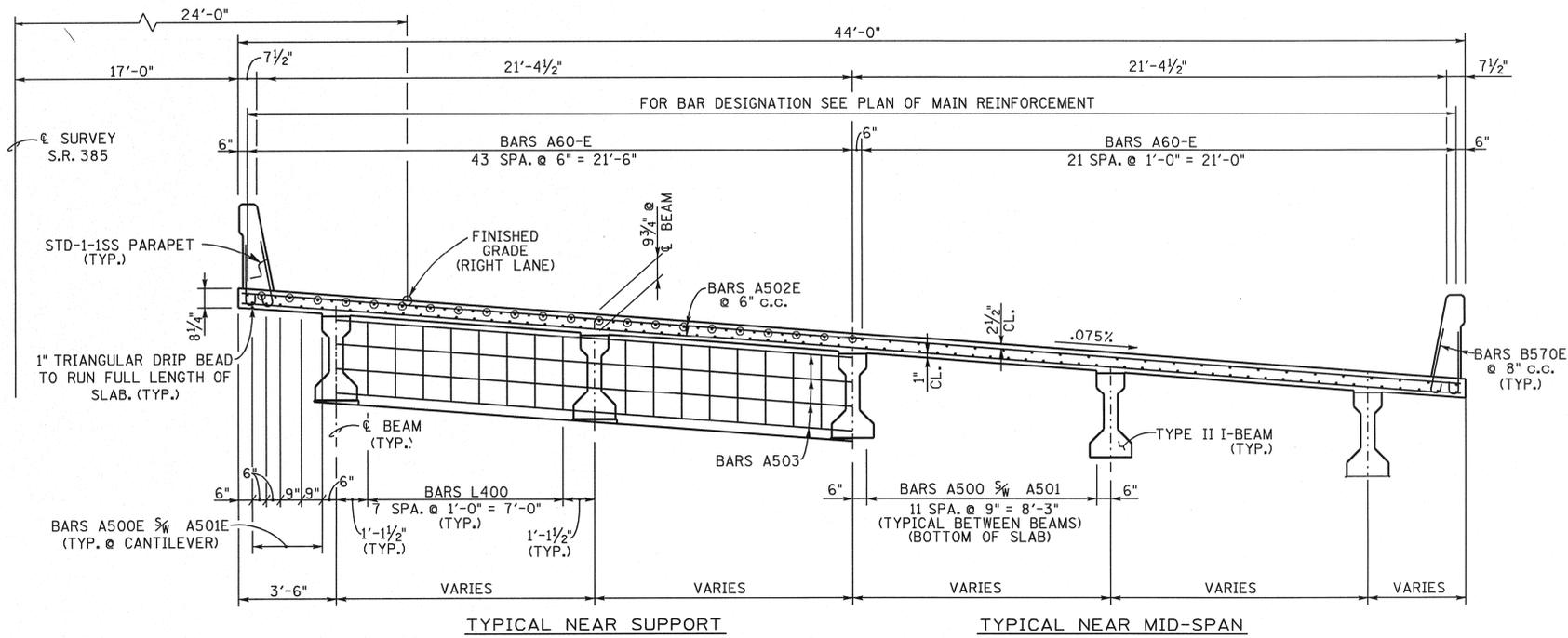
BRIDGE NO. 8 SUPERSTRUCTURE STATE ROUTE 385 (RIGHT LANE) OVER MONTEREY ROAD STATION 364+26.76 SHELBY-FAYETTE COUNTIES 2009



CORRECT Edward P. Wasserman ENGINEER OF STRUCTURES

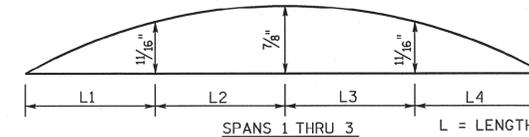
DESIGNED BY K. McALISTER DATE 10-01  
 DRAWN BY F. BARTON DATE 8-02  
 SUPERVISED BY FIELDS & WOODS DATE 8-02  
 CHECKED BY DATE





TYPICAL CROSS SECTION  
(LOOKING FORWARD ON SURVEY)

⊙ DENOTES: CUT-OFF BARS

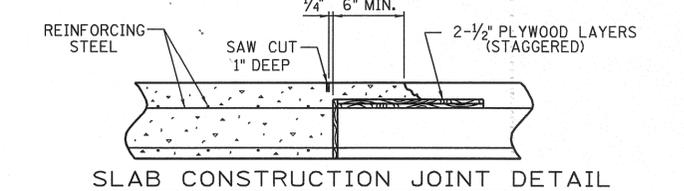


DEAD LOAD CORRECTION CURVE

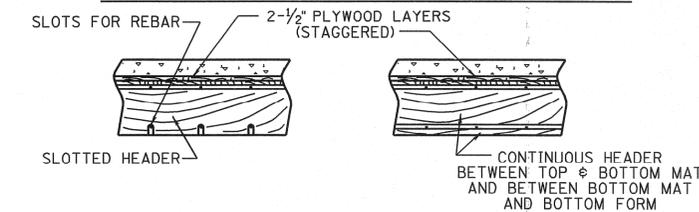
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CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
	2009		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:  
 1. NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.  
 2. THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.  
 3. ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN BELOW.

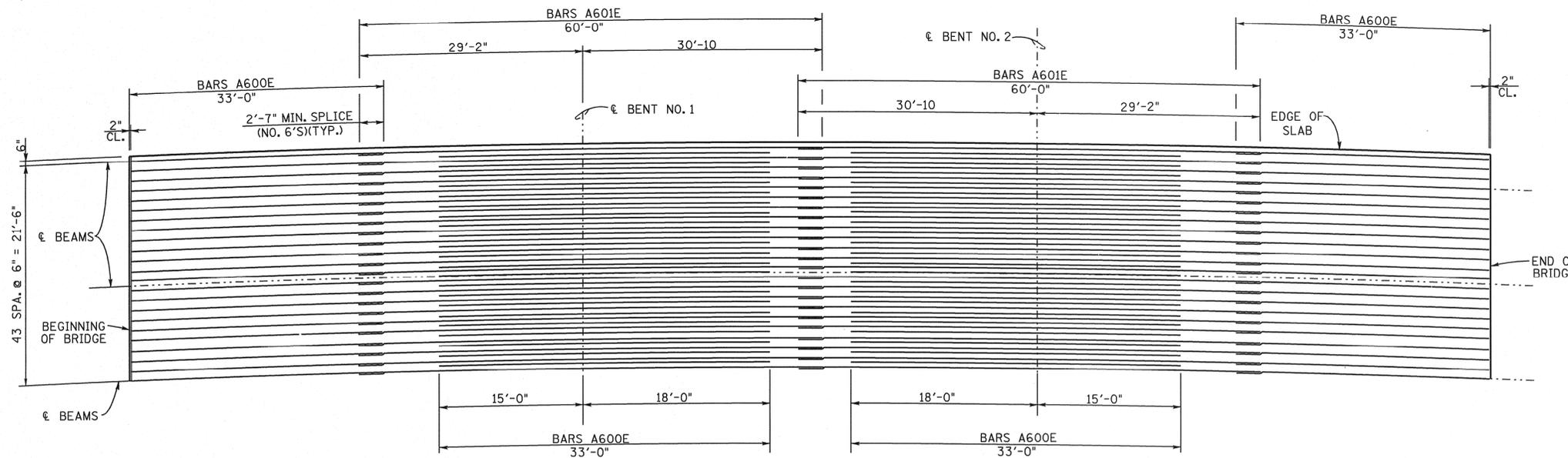


SLAB CONSTRUCTION JOINT DETAIL



ALTERNATE "A" ALTERNATE "B"  
ALTERNATE HEADER DETAILS

**GENERAL NOTES**  
 NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.  
 NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED PRIOR TO PLACING ANY LOADS ON THE GIRDERS HOWEVER, TEMPORARY ERECTION DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.  
 NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE STD. DWG. STD-1-1SS.  
 NOTE: OUTSIDE EDGE OF SLAB AND BRIDGE RAIL TO CONFORM TO HORIZONTAL CURVE.



HALF PLAN OF MAIN REINFORCEMENT

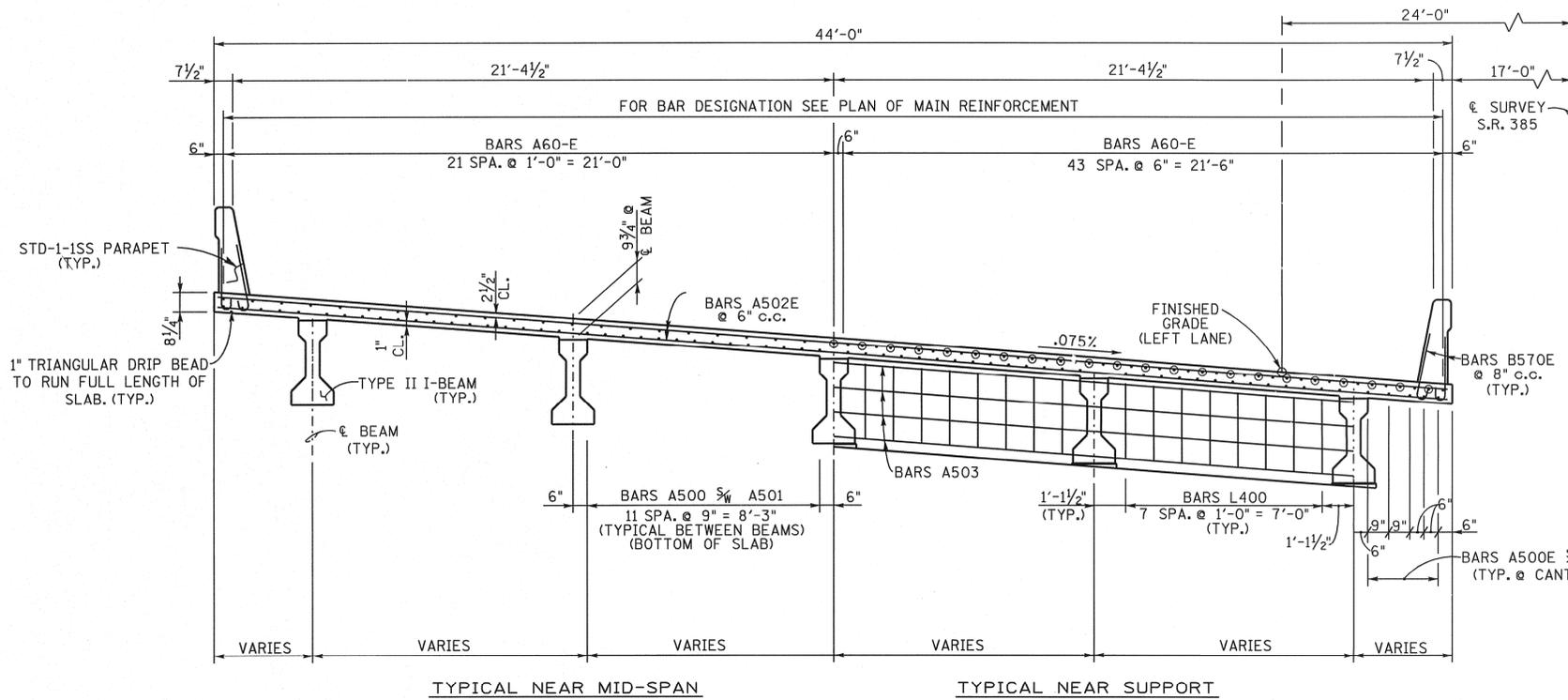
ESTIMATED QUANTITIES		
CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.	REINFORCING STEEL LB.
216	66,508	1,063



STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BRIDGE NO. 4  
 SUPERSTRUCTURE (RIGHT LANE)  
 STATE ROUTE 385  
 OVER  
 WOLF RIVER OVERFLOW  
 STATION 181+50.00  
 SHELBY/FAYETTE COUNTIES  
 2008

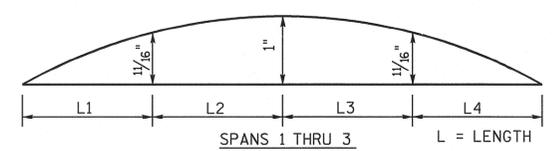
CORRECT *Edward P. Wasserman*  
 ENGINEER OF STRUCTURES

DESIGNED BY J. GUNNELS DATE 03-01  
 DRAWN BY RON CLEVELAND (C.S.) DATE 06-02  
 SUPERVISED BY A.E.P./J.W.F. DATE 06-02  
 CHECKED BY A. PRICE DATE 04-03



TYPICAL CROSS SECTION  
(LOOKING FORWARD ON SURVEY)

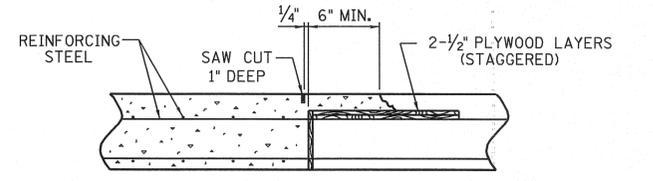
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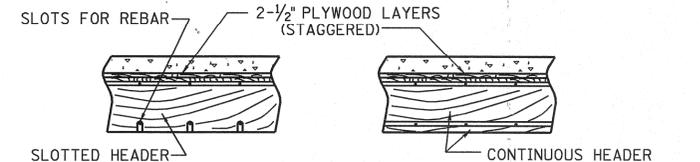
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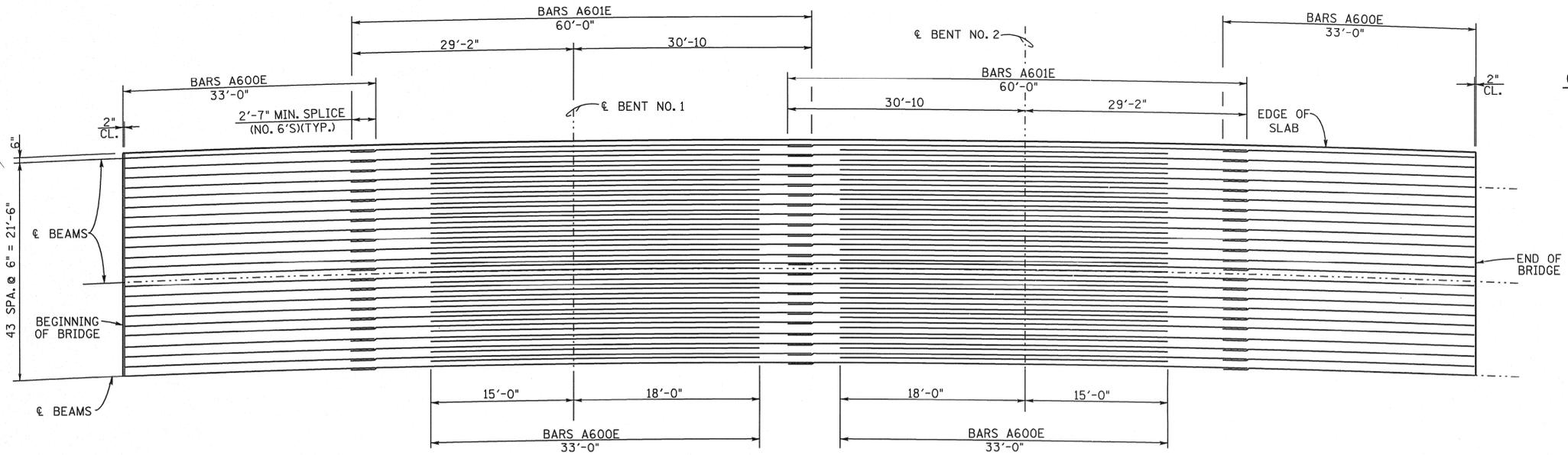
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HALF PLAN OF MAIN REINFORCEMENT



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